



Ultra4 Productions Pty Ltd  
Trading as Ultra4 Australia  
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## Intellectual Property

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## Overview / Introduction

The Ultra4 Australia Rulebook contains the rules, regulations, specifications, and guidelines (hereafter collectively known as the “rules”) that govern the conduct of, and participation in, Ultra4 Australia event.

The Ultra4 Australia Rulebook is intended to provide approved standardised regulations and procedures to ensure the safest, fairest, highest quality competitive motor sports experience for competitors, fans, and sponsors of Ultra4 Australia events.

This Ultra4 Australia Rulebook is issued on the authority of Ultra4 Australia and is effective upon



publication.

All participants in any Ultra4 Australia event must comply fully with all applicable rules as published in this rulebook and in applicable Special Rules and Supplementary Regulations.

Ultra4 Australia may issue Supplementary Regulations that are in addition to the rules contained in the Ultra4 Australia Rulebook, or that amend, suspend or modify existing rules and regulations. Supplementary Regulations apply to a specific event, race, venue, or time and place. Ultra4 Australia may issue Special Rules to account for conditions presented by the location of the event, the condition of the course, or other circumstance. Special Rules and Supplementary Regulations will be considered official amendments or additions to the Ultra4 Australia Rulebook when issued in writing by Ultra4 Australia.

Although this rulebook is directive in nature, no instruction, however comprehensive, can apply in all conceivable circumstances. Nothing in this rulebook, therefore, is intended to replace the requirement for all participants at all times to exercise sound judgment and embody a high level of

competence; nor is it intended to replace the requirement for participants to be responsible for their own safety and conduct.

Equipment standards and specifications set forth in this rulebook, particularly safety standards, are to be considered minimum requirements. This rulebook or any individual rule, specification, or standard set forth herein should not be construed as constraining teams or participants from employing greater safety mechanisms or adhering to stricter safety standards than the minimums required, providing that doing so does not cause a conflict with other rules published in this rulebook.

## Document Conventions

Interpretation of the following words is provided to define their meanings as used in this rulebook:

The word “will” has been used only to indicate futurity; not to indicate any degree of requirement.

The word “may” has been used to indicate an acceptable or suggested means of accomplishment or that compliance with, or application of, a rule or procedure is optional.



The word “should” has been used to indicate that compliance with, or application of, a rule or procedure is preferred or recommended, but not mandatory.

The words “shall” and “must” has been used to indicate that compliance with, or application of, a rule or procedure is mandatory.

Attention has been given to enhancing and clarifying the vocabulary used throughout this rulebook. Specifically, defined terms, acronyms and abbreviations used in this rulebook are defined in the Ultra4 Australia Rulebook - Glossary of Terms, Acronyms, and Abbreviations. This glossary is appended to the Ultra4 Australia Rulebook as Appendix A.

## Terms / Disclaimer / Liability

Ultra4 Australia reserves the right to refuse entry to any applicant, team, competitor, entrant, participant, spectator, or any other person, to any event for any reason.

The reader of this rulebook and all participants in any Ultra4 Australia event hereby agree to waive, release, relinquish, protect, hold harmless, indemnify and defend the promoter, track operator, and



Ultra4 Australia and each of their heirs, successors, officers, officials, employees, agents, and all their respective insurance companies, successors in interest, commercial and corporate sponsors, agents, employees, representatives, assignees, officers, directors, and shareholders of and from any and all claims, demands, liabilities, losses, costs, or damages or expense for any other loss or damage arising, or alleged to have arisen, from any use of any information contained in this rulebook or by reason of any inaccurate information, omission of information, or any negligent act in or related to this rulebook.

Ultra4 Australia does not warrant, represent, or otherwise certify that compliance with the rules contained in this rulebook confers any degree of safety, real or imagined. This rulebook is published without warranty expressed or implied.

The reader of this rulebook, all participants in any Ultra4 Australia event, and any user of any safety device assumes any all risks involved with the use of any information contained in this rulebook, with their participation in any Ultra4 Australia event, and with the operation of a vehicle.

Ultra4 Australia is not a professional engineering company, safety expert company, or medical



professional company. Ultra4 Australia does not represent any manufacturer of safety equipment nor does Ultra4 Australia warrant or endorse or represent that anything written in this rulebook is, in any way, shape, or form, fit for any purpose whatsoever.

Nothing written in this rulebook is intended to be professional, competent or qualified advice on how to design, build, fabricate, install or use any vehicle, component, part, device, system, or piece of equipment, including safety systems.

No warranty or representation is made as to the ability of any of the information contained in this rulebook to protect any reader of this rulebook, any participant in a Ultra4 Australia event, or any user of any vehicle, part, system, or safety device (whether mentioned herein or not) from injury, property damage, or death.

By participating in any way in any Ultra4 Australia event all participants signify that they understand and agree that participating in a Ultra4 Australia event, installing or using any safety device, and operating an off-road vehicle for any purpose CAN BE HAZARDOUS AND PRESENTS A RISK OF PROPERTY DAMAGE, PHYSICAL INJURY OR DEATH.

All participants expressly ASSUME ANY AND ALL RISKS associated with using any information published in this rulebook, using any driver restraint or other safety system, or participating in any way in any Ultra4 Australia event, whether those risks are known or unknown, inherent or otherwise.

Ultra4 Australia assumes no responsibility for decisions made by individuals or others using this rulebook.

Ultra4 Australia assumes no responsibility whatsoever for delays, postponements or cancellations of all or part of an event for any reason, including inclement weather or unsafe course conditions.

Ultra4 Australia event participants, officials, and volunteers are not employees of Ultra4 Australia. Ultra4 Australia event participants, officials, and volunteers assume all responsibility for all charges, premiums, and taxes payable on any monies, prizes, or other awards that they may receive as a result of their participation in any Ultra4 Australia event.

## Precedence

In the event of any inconsistency within this rulebook, Ultra4 Australia shall be contacted for



clarification prior to the start of an event.

In the case of a discrepancy noted, or clarification required, after the start of an event, Ultra4 Australia will use the post-race Competition Ruling Committee (CRC) to determine the appropriate response. Such response may include, but need not be limited to: making a ruling, issuing a clarification, taking disciplinary action, or taking some other action deemed necessary by the CRC.

In the event of conflict between the contents of this rulebook and the rules or specifications of any outside sanctioning body with whom an event is co-promoted, the stricter or more stringent rule or specification shall prevail.

In the event of any inconsistencies between the contents of this rulebook (including any documents referred herein) and any applicable Australian federal, state, or local laws and regulations, the latter shall prevail.

In the event of conflict between any documents referenced herein and the contents of this rulebook, the latter shall prevail.

Ultra4 Australia reserves the right to modify this rulebook, at any time, at their sole discretion.

## Vehicle Requirements (All Classes)

### Initial Scrutineering

All vehicles in all classes must be scrutineered and passed by a registered Ultra4 Australia Scrutineer prior to starting their competition season with Ultra4 Australia.

The owner then must apply to Australian Auto Sports Alliance for a vehicle logbook.

The vehicle will then be issued with an Australian Auto Sports Alliance (AASA) logbook which will have details of the vehicle and must be present at every Ultra4 Australia race.

## Equipment Condition and Function

### Serviceability

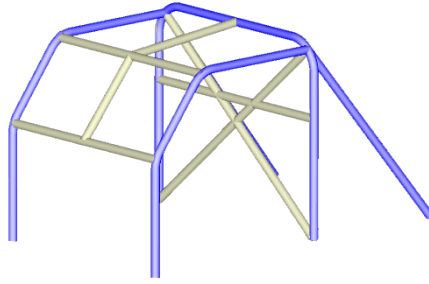
All necessary or required equipment, gear, devices, safety equipment, and vehicle components, as described in the Ultra4 Australia Rulebook (including any special rules or supplementary regulations),



must be in good and proper working condition at the time of technical inspection. Certain equipment and components must remain serviceable throughout the event, and if damaged during the event must be repaired or replaced before the vehicle may continue on course, as specified in the Ultra4 Australia Rulebook Technical Rules and Specifications.

### Rollcages (Individual Class Rules May Supersede These)

1. Six (6) point mounting cages are required over the occupants.



2. It is each competitor's responsibility to present a safe vehicle for pre-event technical inspection. Competitors must maintain their safety equipment including the roll cage integrity. Ultra4 Australia reserves the right to not allow any cage designs that, in the opinion of the Chief Technical Inspector, is not fit for competition. Competitors are ultimately responsible for their vehicle's safety features, including the design, fabrication, quality of execution, maintenance and repair of the roll cage structure. The roll cage is considered to be the main 6-point structure that surrounds and protects the vehicle's occupants.



3. All vehicles must be equipped with a roll cage fabricated to a standard to meet the industries (or outlined herein) minimum requirements.
4. Rollcage main structure material may be ERW, DOM or CDS mild carbon steel (manufactured to ASTM-A513 standards) or 4130 chromoly steel. All welds must be of high quality and craftsmanship with good penetration and with no undercutting of parent material. All Chromoly used must be scrutinised before paint is applied, either in a face to face inspection or thoroughly by visual documentation. Minimum tensile strength for roll cage components is >350MPA.
5. Main 6-point requirements for ALL classes of vehicle (Refer Diagram A1), must be made from a minimum material size of 44mm x **3mm**. All other structural chassis steel may be made from a minimum material size of 38mm x 2.5mm. **As of all builds started after Feb 1 2019.**

*N.B - Ultra 4 & Ultra4 limited class require a minimum of 44mm x 3mm for vehicles under 2 tonne and 50mm x 3mm for all vehicles over 2 tonne.*

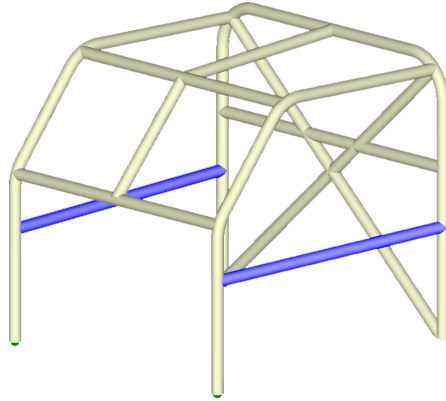
*N.B - This rule applies to all Ultra4 & Ultra4 limited class cars starting to be built after Jan 1 2021*

- 6.** All roll cage components (hoops, braces, gussets, etc.) must have a minimum of 50mm of clearance from any vehicle occupant's helmet when occupant is seated in normal driving/riding position. All roll cage components that might come into contact with the vehicle occupants' helmets must be padded with a fire proof padding material.
- 7.** Rollcages must be securely mounted to the frame, chassis, or body. Roll cage terminal ends must be attached to a frame or body member that will support maximum impact and not shear or allow movement in the cage terminal end. Cab/body-mounted roll cages must sandwich the body structure using a minimum of two 3mm thick, dissimilar sized, double plates, one on each side of the body structure that have a surface area of no less than 100mm x 100mm Square. Roll cage mounting fasteners must be at least 10mm diameter Grade 8 or



equivalent or better. Sandwich plates, if used, must be oriented only in the horizontal plane. No vertical or other non-horizontal sandwich plate orientations are permitted.

- 8.** All vehicles excluding those with stock steel doors, must have at least one side bar on each side of vehicle that will protect occupants from side impact. The sidebars must be of the same tubing material and dimensions as the main frame of the roll cage. The sidebars should be as close to parallel to the ground as possible, must be located to provide maximum protection to the occupants, and Ultra4 Australia recommends side bars be securely welded to the front and rear hoops of the roll cage. The location of the sidebars must not cause difficulty in entering or exiting the vehicle. Modified class vehicles will also be allowed to have bolt in side intrusion bars to meet with other event class rules. These bolt in bars will have to meet the requirements of Ultra4 Australia' rulebook and chief technical inspector's discretion.



9. A minimum 1mm expanded or flat sheet mild steel or 3mm aluminium must cover the area

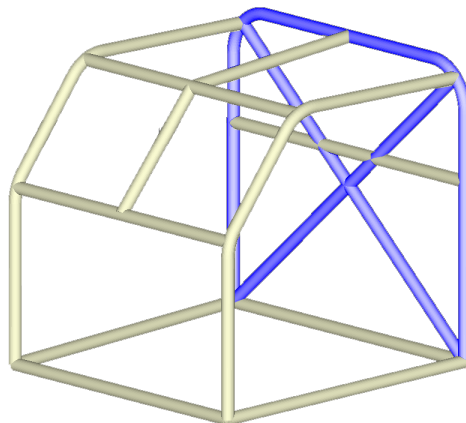


immediately above the occupants' seats and be attached via welding or bolting to a steel tubing frame work.

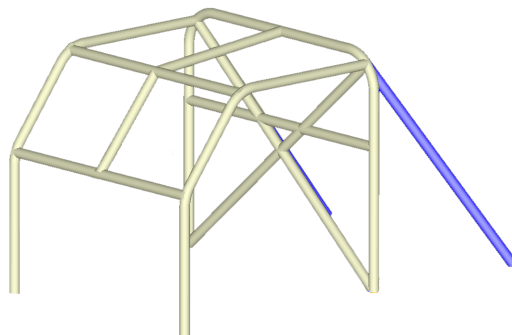
10. All rollcages must be subject to the inspection of a registered Ultra4 Australia Scrutineer prior to attaining a chassis identification number.
11. The roll cage frame must be constructed so that it: Reduces the deformation of the occupant space in an impact whether it be from rear, side, front or top/bottom intrusion; Each join on cage material shall be welded fully around the adjoining metals (intersections with more than one tube join should have the first joints fully welded first); Gusseting on all major joins is highly recommended; The frame must not impede the entry or exit of occupants and must have a minimum of two exit points. In the event of a roll over, Ultra4 Australia recommends occupants be able to exit the vehicle within 10 seconds.
12. Tubes forming part of the 6 Point Safety Cell must not contain or be a vessel for any fluids.

## Mandatory Roll Cage Design

1. Each roll cage structure/chassis must incorporate the following tube work as a minimum specification.
2. A main hoop "B pillar" with two diagonal braces. Diagonal braces must be attached within 100mm of the top outer bends of the B pillar on both sides of the vehicle. Diagonal braces must have their lower ends terminated at the intersecting joins of the Bottom chassis rail/tube and the B pillar. **Minimum material size 44mm x 3mm**



3. Two rear stays or “sixth points”. Rear stays must be attached in lateral line with the roof tube “A” Pillar on both sides of the vehicle. They shall be at an angle of no less than 30 degrees to vertical and shall terminate at either the chassis, or sandwich plated onto the body panels with a minimum of 2 x 3mm plates of an irregular size. The rear stay must be straight with no bends. It must run to the lowest part of the rear chassis in a straight line or be connected to the lowest rear part of the chassis via another tube. The main B pillar may comprise of more than one section of tube, as long as the intersecting joints are sufficiently gusseted. Ute bodies must still incorporate the rear stays whether by cutting holes to allow the stays to pass through or engineering flanges to sandwich the body itself. On application to Ultra4 Australia a second external B pillar may be used to incorporate the rear stays.

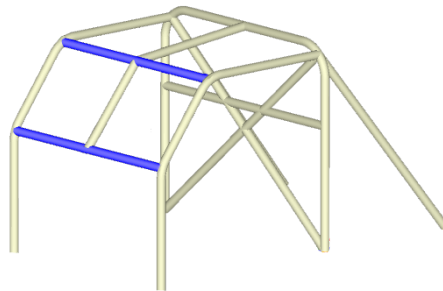


4. An “A pillar” (front leg) which runs into one complete lateral tube (roof bar) to connect with the B pillar. The A pillar should be minimised to two bends, one being at the intersection of the roof spreader and the other at the intersection on the dash spreader. The vertical legs of the A pillar must be straight and welded either directly to the chassis, or sandwich plated onto the body panels with a minimum of 2 x 3mm plates of an irregular size. No part of the

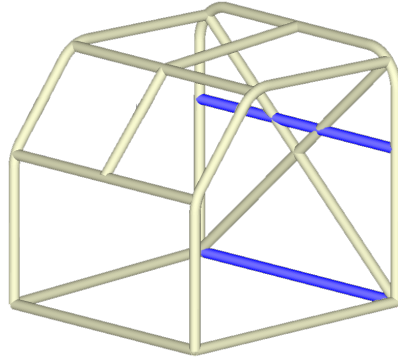


interior trim of a vehicle should compromise the positioning of the vertical portion of the A pillar. At the top outside front edge of the A pillar (ie, the top of the windshield), the A pillar is recommended to be sufficiently gusseted to prevent deformation in the event of a roll over. The A pillar may be joined directly to the rear diagonal backstays providing the join is recorded in the logbook, sleeved with a correctly fitting tube and gusseted sufficiently.

5. A roof spreader bar and a dash spreader bar must be incorporated into the cage design.



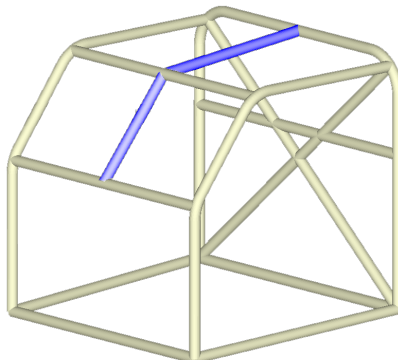
6. A Rear Cross member must connect the feet of both left- and right-hand side B pillar feet. This cross member must be joined to the B pillar no less than 100mm from the intersection of the B pillar and its diagonal bracing. These braces must be straight with a minimum tube size of 44mm x 3mm.
7. A Harness bar shall be placed according to section 7.3.7. The harness bar may comprise of more than one section of tube to intersect with the two diagonal cross braces of the B pillar. Minimum size of tube 38mm x 2.6mm



8. Roof bracing shall consist of at least one longitudinal and two diagonal members between the main A and B pillars. Further triangulation bracing is recommended by Ultra4 Australia providing there is sufficient occupant headroom as per section 7.2.6. Minimum tube size of 38mm x 2.6mm



9. Windscreen bar options are open to discussion. At a minimum, one piece of 38mm x 2.6mm tube is recommended to span the gap between the dash spreader and the roof spreader bars. A 2 bar option can be used with a minimum tube size of 25.4mm x 3mm



10. Additional tubework is allowable, providing it does not intrude on the window openings and the safe exit of the vehicle by all occupants. Ultra4 Australia highly recommends the use of further side intrusion tubework, further rear stay cross bracing, A pillar bracing, preventing internal crushing in the form of wind wing bracing, and any further tube work implemented to triangulate the minimum requirements.
11. **All vehicles built after 1st February 2019 or All vehicles competing after January 1st 2020 must have a "A" pillar support that runs from top of "A" Pillar roof bar to floor area. This may be one or two piece in design.**

### Driver Restraint Systems (Also Refer To Supplementary Class Rules)

1. All vehicles must have a minimum five-way, five-point driver restraint system for each occupant. Restraints must incorporate a lap belt, and shoulder straps as a minimum and also be compliant with ADR AS2596 or SFI 16.1 or internationally recognised equivalent. **All seat belts have a 3 year life span from date of purchase (receipt required) if a receipt cant be**



2. The driver restraint system shall consist of at least one 2" wide lap belt and two 2" wide shoulder straps. Sternum straps and chest buckles may be used. 3" straps are recommended for your comfort.
3. Belt/strap material shall be nylon or Dacron polyester. Driver restraint system must be in new or perfect condition with no cuts, frayed layers, chemical stains, or excessive dirt and must be in flexible condition (i.e. material must not be stiff). All driver restraint systems must show the manufacturer's name and the month and year of manufacture.
4. No portion of the driver restraint system may be altered in any fashion from the manufacturer's standard design.
5. All driver restraint systems must be properly mounted in accordance with manufacturer's directions and recommendations. Bolt-in, wrap-around, and snap-in mounting styles are

permitted, providing the mounting style meets the equipment manufacturer's requirements, along with the Ultra4 Australia own requirements. The ideal position is 90° to the occupant's spine when occupant is seated in proper driving position but up to 15° below the driver's shoulder is acceptable. Seatbelt must not be any more than 20deg to occupants spine when seated in proper driving position.

6. In addition to conforming to the manufacturer's directions, driver restraint system installations must also conform to the following:
  - a. The driver restraint system must be mounted to structural members able to withstand the load the restraint system will place on them in a crash, without rupturing or failing.
  - b. Driver restraint must be matched to a properly constructed, fitted, and installed seat firmly mounted to the frame / chassis / roll-cage.
  - c. Driver restraint system must be used with a seat with the proper number of slots, in the proper locations, for the belts. Seats must not be modified to create belt slots.



- d. All belts should be as short as possible to minimise the belt's stretch.
- e. Belt routing must allow webbing to pull in a straight line against anchor point. Mounting brackets must be at an angle that is compatible with the direction of pull on the webbing.
- f. Preferred anchor mount is a double-shear bracket
- g. Driver restraint systems must be mounted using high-quality hardware appropriate for the installation. 10mm fine-thread Grade 8 bolts , 7/16 UNF and Grade 8 deformed-thread locknuts (or better) are recommended.
- h. Belts must not rub against any surface that will cause them to fray.
- i. Wrap-around style mounting must be confined to shoulder belt installation and must include some method to prevent lateral movement of the belts.
- j. Driver restraint systems must be worn properly tightened, by all occupants, at any time the vehicle is in motion (Including transition stages, Pit lane, Pre-Running, Verification and Impound movements – If it moves under power, you need to be strapped in!)

- k. All vehicles must have a commercially available seat belt cutting device for each occupant. This must be mounted on the A pillar in such a way that it can be easily removed by occupant or by someone external of the vehicle and of a design that poses no cutting risk to the occupants. If occupant cannot reach this A pillar mounted cutter then a second cutter for the occupants must be placed within occupant's reach.
- l. Only latch and link or camlock style latches are allowed. No car seat belt style latches are allowed.

### Safety Nets

1. Ultra4 Australia approved safety nets are mandatory on all vehicles and must cover the complete open area of the cockpit on both sides of the vehicle to the extent that it is impossible for any limb or body part of any occupant to protrude from the vehicle at any time when the occupant is properly seated and strapped in their normal driving / riding position. All nets must be made from nylon that the web gap is no more than 50mm x 50mm.
2. Wind wing areas located behind the A pillar must be filled by safety net material - or a high



resistant plastic – IF, there is any chance that any limb or body part of any occupant could protrude from the vehicle at any time when the occupant is properly seated and strapped in their normal driving / riding position.

3. Nets must be installed on the inside of the roll-cage or body to prevent them from being damaged or coming off in a roll over or slide on the side.
4. Nets attached to door frames are permitted.
5. Nets must be installed so that the occupants can release the netting unassisted and exit the vehicle regardless of the position of the vehicle.
6. The net border or edge and the net attachment must be made of materials that are as strong as, or stronger than, the net itself. Attachments include, but are not limited to: steel hose clamps, snaps, metal hooks, and steel rods. Zip ties are not permitted for net attachments.

Nets must be tight so that when subject to a pushing force of approximately 20kgs the net deflects no more than 10cm, or half a helmet.

### Fire Extinguishers

1. Each vehicle must carry at a minimum 3 x 0.9kg or 1 x 1.8kg portable, approved ABC-class dry chemical or equivalent fire extinguisher. Fire extinguisher/s must have a gauge, be fully charged, and be easily accessible from inside of the vehicle by both driver and navigator as well as easily located and accessed from the exterior of the vehicle by persons not familiar with the vehicle. All external extinguishers must be mounted on or as near as possible to the vehicle "B" Pillar and be accessible by persons outside the vehicle.
2. All extinguishers must be mounted in a manner that permits their removal and use without the use of tools.
3. All fire extinguishing/suppression systems must have a current expiry date affixed. Failure to present this standard may result in the extinguishers being rejected at technical inspection.



All extinguishers are only valid from 3 years of manufacturers date as stamped on the extinguisher.

### Seating

1. All seats must be manufactured by a recognised manufacturer that specialises in seats for racing applications and be of a type suitable for the event.
2. All seats must be securely mounted to frame of vehicle and mounts must be properly reinforced to keep seat from moving in relationship to the frame. Stock and modified class, seats may be mounted to OEM locations, providing the areas are free from damage and the correct hardware is used.
3. Adjustable track-type seat mounts must be securely mounted to frame of vehicle to allow no lateral or vertical movement between seat and frame or mounting track and frame.

4. Headrests constructed of at least 50mm thick resilient padding and being approximately 20cm square in area are required for seats not made from the factory with sufficient headrests.
5. Seats must have appropriate slots to properly accommodate driver restraint system.
6. All seats must be of a fixed back style of seat. No reclining seats

### Sirens

1. All vehicles must be fitted with a loud Siren. Sirens must be clearly audible from 30 meters in front of the vehicle. The use of horns is not permitted. Disposable air horns are not an acceptable method of meeting the horn requirement.

### Reflectors

1. All vehicles must have two 50mm wide x 200mm long red reflective tapes or two 50mm diameter round red reflectors (DOT stock taillight lenses meet this requirement) attached to the rearmost portion of vehicle at each corner. The reflective tape or reflectors must be



clearly visible from the rear.

### First Aid Kit

1. A weatherproof first aid kit must always be carried in each vehicle and must contain at least basic first aid items. The first aid kit must be easily accessible within the occupant's area without having to remove any body panels or equipment. Occupants with special medical needs should make those needs known in an obvious location on their fire-suit or helmet.

### Survival Supplies

1. For any events with a single lap of a track longer than 50 km, it is recommended that all vehicles carry water, food, any medications and any other supplies required for the vehicle occupants to safely survive unassisted for a period of not less than 24 hours. As a minimum, at least two litres of water per occupant must be carried. It is highly recommended that additional water be carried for each occupant during hotter weather. Ultra4 Australia highly recommends entrants carry sunscreen. Ultra4 Australia also recommends all occupants carry the minimum water requirements at events less than the above stated distance.

## General Vehicle Components

### Vehicle Identification

1. All vehicles in competition must be identified with the correct entrant number.
2. Entrant numbers shall be composed of a combination of the digits 0 through 9 only.
3. Entrant numbers shall be assigned annually to Drivers of Record on a first-come first-served basis. In the event of a conflict, seniority based on the date on which the Nominated Series Driver first competed in a Ultra4 Australia event will determine the outcome.
4. Vehicles must display entrant numbers on front, both sides, and back of vehicle. Any number location that is deemed by Ultra4 Australia to be too hard to read will have to be changed before vehicle competes in the event.
5. Colour and shape of numbers is at the discretion of vehicle owner. The minimum size for



vehicle numbers must be 150mm in height and 90mm wide (per character). Entrants are advised that checkpoint staff at each checkpoint will not allow vehicles to continue after stopping, until numbers can be verified.

6. Ultra4 Australia reserves the right to require race vehicle numbers and/or background colours be changed.
7. Ultra4 Australia assumes no responsibility for scoring vehicles with unrecognisable numbers. It is the vehicle driver's responsibility to maintain numbers in recognisable condition.

### Vehicle Body

1. Firewalls and/or bulkheads must separate the driving compartment from any fuels, engine fluids, and acids.
2. Oil coolers, transmission coolers and radiators located in front of the vehicle occupants must have a shroud that, in the event of a rupture or leak, will prevent liquids from blowing back



or leaking onto the occupants. All hoses running through the passenger compartment must be shielded. Steel braided hoses do not constitute a shield.

3. All vehicles with rear mounted radiators or coolers must have a deflection shield mounted that will shield vehicle occupants from high pressure fluids in the event of a hose or line failure.
4. All vehicles with operational doors must have positive locking mechanisms on the doors.
5. All vehicles must have an all-metal firewall separating the occupants' compartment from the danger of fire from the engine and fuel supply. A minimum firewall must be sealed and extend from body side to body side. If engine is rear-mounted, firewall must be sealed and extend from the driver's shoulder height to the vehicle floor and extend from body side to body side. If rear mounted fuel cell or radiator is higher than driver's shoulder height, a firewall between the driver and the fuel cell must extend at least 50mm above the top of the fuel cell. Any hole placed in the firewall for structural members, lines, etc. must be kept to a



minimum. The hole should not have more than 3mm gap around the items passing through the firewall. Metallic tape or rubber grommets must be used to seal the hole between the firewall and the item passing through the firewall. Rear mounted engines are not required to have a top mounted hood.

6. Floorpans are required on all vehicles. Floorpans must cover the entire area from in front of the pedal assembly to behind the seat(s), and from the outside edge to the outside edge of the vehicle. Installation must be done in such a manner as to afford maximum protection to the occupants from debris.
7. All vehicles must start event with a functional: generator or alternator, fan, water pump (water-cooled vehicles), and a complete functional electrical system. Air cooled vehicles are permitted.
8. All protruding bar work must be safe. No hazardous front or rear bumpers, nerf bars, frame heads or other protruding objects from vehicles are permitted. Ends must be capped and

rounded to prevent any sharp edges. Bumpers and nerf bars must be designed in a way that reasonably minimises the chance of two vehicles becoming locked together – regardless of whether the class of vehicle allows for nerfing and/or bumping within the race conditions.

9. A rear-view mirror is required on all vehicles. Mirror must have a reasonably unobstructed view of area behind vehicle.
10. Bash plates designed to afford a reasonable degree of protection to the front suspension, steering, and brake components are recommended on all vehicles. Bash plates must be securely attached.
11. All spare parts and extra equipment carried on or in a vehicle must be securely attached or stowed to prevent movement during competition. All spare parts and extra equipment must be carried in a manner that minimises the risk of injury to the vehicle occupants.
12. All vehicle body parts must remain on the vehicle (accidental damage excluded) during the



entire event.

## Engine

1. Engine shall be free of leaks.
2. Engine vents shall run to a fluid containment system mounted externally of the occupants seating area. Dipsticks are recommended to be of the locking type.
3. Exhaust system outlet must extend a minimum of 300mm past the rear of the occupants' compartment; be directed out of the body away from the occupants, fuel cells and tyres.
4. All entrants are entitled to replace a complete engine during an event. Prior permission will need to be sought from the CRC to ensure that a replacement engine is of the same bore, stroke capacity and induction as

## Transmission

1. Transmission shall be free of leaks.
2. Transmission vents shall run to a fluid containment system mounted externally of the occupants seating area. Dipsticks are recommended to be of the locking type.
3. Every vehicle must have a functional reverse gear.
4. Transmission shall have an approved scatter shield, or approved floor between occupants and transmission.

## Transfer-Case

1. Transfer case shall be free of leaks.
2. Transfer case vents shall run to a fluid containment system mounted externally of the occupants seating area.



3. All vehicles must be capable of transmitting power to all four wheels/tyres, and must be equipped with a functioning low range. Low range is defined as a gear ratio that is lower (numerically higher) than 1:1.

## Steering

1. Power-assisted steering systems shall be free of leaks.
2. Power-assisted steering vent tubes must be attached to a fluid containment system which prevents any fluid from leaking onto the ground.
3. Drag link and tie rod ends designed for use with a castellated nut and cotter pin must be secured with a cotter pin. Spherical rod ends (Heim joints) are a permitted replacement for OEM-style tapered tie rod ends.

4. All hydraulic steering lines must be in good working order and free of cracks, defects, or leaks. Hydraulic lines shall be run in a manner that protects them from possible damage.
5. All hydraulic steering lines must be shielded from all occupants in a manner that prevents accidental rupture onto any part of the vehicle's occupants.
6. No vehicle steering wheel may comprise of wood.

### Suspension

1. There must be at least one shock absorber per wheel.
2. Suspension pivot points and connecting points must be free of cracks and in good physical condition as determined by the Chief Technical Inspector or his/her delegate.
3. Shock absorbers shall be free of leaks.

### Brakes



1. Brakes must be able to apply adequate force to lock up all four tyres. Brakes must be in a safe operating condition and free of leaks during the entire event. If brake system problems occur during the event they must be repaired before continuing in competition.
2. Turning, cutting, or steering brakes are permitted.
3. Manual, vacuum boosted, and hydraulically assisted brakes are permitted.
4. Transmission and/or pinion-brake systems are permitted, providing they meet all other requirements specified herein.
5. Each vehicle shall have a means of applying continuous brake pressure (hand brake) while vehicle is parked with occupant(s) outside the vehicle. Hydraulic "line-locks" or mechanical "emergency brakes" are permitted.

## Controls

1. All throttles, whether controlled by hand or foot, must have at least one return spring of sufficient stiffness to instantly close the throttle plate when the throttle is released. Carburetted vehicles must have at least two throttle-return springs, at least one of which must be attached to the carburettor. Computer controlled throttles (Electronic Throttle Control or “drive-by-wire” systems) are exempt from the requirement to have a return spring at the throttle body but must have a return spring at the throttle control (pedal or hand control) or maintain the stock OEM system.

## Recovery Items

1. All vehicles must have securely stowed during the event the following recovery items at a minimum:
  - Snatch Strap
  - 2 x Rated D Shackles (3.25t minimum)
2. All recovery equipment (including winch line) must be in safe operational condition. All



recovery items will be inspected at Technical Inspection and entrants will DNS if they do not meet the safety requirements herein.

3. All entrant’s vehicles and any pre running vehicles must have suitable recovery points. These points must be constructed with a 10mm thick plate with a minimum tag of 75x75mm and be either fixed the chassis with a minimum of three grade 8 16mm bolts per point or welded with sufficient penetration. Each vehicle must have a minimum of two recovery points both front and rear and all points must be painted bright red as per the industry standard, or in the event that body or chassis colours are red, a bright, contrasting colour.

## Fuel System

### Fuel: Types

1. Any of the following commercially available fuels may be used:

- Service station pump gasoline (the type normally used in passenger vehicles for highway use)
- Racing gasoline, as originally manufactured
- Commercial aviation gas
- Diesel
- Alternative fuels, including bio-diesel, on approval of Ultra4 Australia
- Propane, LPG or natural gas. Entrants running LPG styles of systems will have to have a current LPG certificate and the tank shall be within the test date. Tanks must be mounted using steel braces/clamps.
- Commercially produced, nationally advertised fuel additives may be used.
- Nitrous Oxide is permitted as per class specific regulations. Nitrous Oxide must be securely mounted and installed as per the manufacturer's specifications and requirements and must be inspected and passed by Ultra4 Series Scrutineers. Ultra4 takes no responsibility for Nitrous failure, engine failures or accidents arising from the use of Nitrous Oxide.



2. Alcohol and nitromethane are not permitted.

#### Fuel: Storage

1. **Safety fuel cells are required for vehicles competing in Ultra4 & Ultra4 Limited class. These must be a double banded style fuel cell with an internal bladder/tank and an outside skin. All other vehicles can run OEM fuel tanks in their original OEM configuration.** OEM Fuel tanks from other makes and models of vehicles are permitted, but all OEM style fuel tanks MUST retain factory style filler necks (with locking caps), supply lines and return lines. **These tanks must have bash guards fitted to the bottom of the tank.**
2. Auxiliary fuel tanks may be added (including surge tanks). Auxiliary fuel tanks must meet the Ultra4 Australia safety requirements and be safe to contain fuel under pressure and free of leaks in any vehicle orientation.
3. There must be a substantial firewall between the fuel tank and the occupants.

4. Fuel tanks shall be mounted in a fashion to protect the tank from damage due to a rear-end collision, impact from debris or rocks from below the vehicle, damage due to roll over, or the possibility of damage from chassis flex.
5. Fuel Container must be securely attached to vehicle with bolts or steel straps. All fittings must be built into the container skin and bonded to the container skin as an integral part of the tank or mechanically sealed by a ring and counter-ring system by either flat joint or an O-ring. Internal baffling, foam or bladders are mandatory if fitted with fuel cells. Rotary moulded polymer cells are acceptable when encapsulated in suitable steel retaining straps.
6. Fuel surge tanks (accumulators/surge tanks) are permitted under the following guidelines. Accumulator tanks shall be mounted to the chassis using rubber isolation. Accumulators shall be mounted in a manner that protects them from damage due to impact.
7. No jerry cans or other portable fuel containers shall be permitted in or on any entrant vehicle during the event. Use of jerry cans or other portable fuel containers will subject entrant to a



8. Alternative fuels (propane or natural gas) must use an approved fuel cell as determined by DOT standards and with the approval of Ultra4 Australia Forklift propane fuel tanks are permitted.
9. **Alloy tanks are not allowed in Stock, Modified or Ultra4 Classes**

#### Fuel: Plumbing, Filling & Ventilation

1. Design and installation of fuel tank and related components (plumbing) must prevent fuel escaping from fuel pickups, fuel lines, fuel fillers and fuel vents if the vehicle is partially or totally inverted.
2. Fuel tank must be filled from, and vented to, the outside of the occupants' and engine compartment/s.

3. Fuel filler lines and positive-locking, non-vented fuel filler caps must be located and secured in such a manner as to prevent them from being knocked off or open during vehicle movement, rollover, or accidental impact. flip-type caps are strictly prohibited.
4. All fuel fillers attached to the frame or a body panel must be connected to the tank using flexible couplers. All fuel fillers must be surrounded by a boot or splashguard (body panel is acceptable as a splashguard, if it is sealed). Boot or splashguard must direct fuel spillage to outside of vehicle and away from occupants' compartment, engine, and exhaust. It is highly recommended that detachable fuel filler caps have a flexible strap or chain to secure them to the vehicle.
5. Fuel vent lines must have a rollover check valve incorporated at the fuel cell, and must vent outside of occupants' compartment, and be directed away from the engine and exhaust system.
6. Fuel vent line must use one of the following two routings:



7. Fuel vent line must extend to the highest point of the roll cage nearest the fuel cell, across the width of the vehicle, and down to below the belly pan of the vehicle or 75mm below the fuel cell, whichever is lower.
8. Fuel vent line must loop above the fuel cell to a point that is 100mm above the top of the fuel cell. From there it shall be wrapped one full loop around the outside of the fuel cell near the top of the fuel cell and then be routed down to a point 75mm below the lowest point of the fuel cell.
9. Fuel mats are required for all refuelling. No vehicles shall be refuelled outside approved pit locations. Storage of fuel in the pits shall consider safety the highest priority. All fuels MUST be stored in an approved clearly marked and identified safe container. It is highly recommended that a red plastic jerry can be used for Unleaded/Leaded petrol and a Yellow plastic jerry can be used for diesel. Ultra4 Australia highly recommends the use of safety tape



and “No smoking/No open flame” signs in the area surrounding fuel storage and transfer locations.

## Fasteners

1. All component parts of the vehicle’s steering, suspension, chassis, drivetrain, and running gear be secured with Grade 8 or better, fasteners. Male threaded fasteners (bolts, cap-screws, studs, etc.) should be secured with either: lock nuts, lock washers, cotter pins or safety wire and shall have at least one full thread showing through the nut.

## Electrical System

### Communication Equipment

1. All Competitors race vehicles must have a minimum of a 5 watt fixed mounted UHF radio capable of transmitting on any frequency channel from 1-80 if required. Hand held UHF’s are not permitted as the sole communications in competing vehicles, they are allowed as a secondary communication device if required for crew or navi communication.



2. All vehicles using the course, including recovery crews, pit vehicles, media, entrant vehicles and all any other team members, must have a functioning 5 watt or greater UHF radio. This may be in the form of a hand held unit, Ultra4 Australia would prefer a fixed mounted unit.
3. All vehicles traversing the race course at any time must be tuned into the official UHF race channel designated by Ultra4 Australia Failure to follow this regulation may result in the matter being brought before the CRC either before or after an event has been run.
4. Other forms of radios may be used for team communication and all relevant users must have a current license if they are required to do so to operate these.

### Kill Switch

1. A brightly coloured, highly visible, easily distinguishable, master kill switch must be located in the dashboard area of the vehicle and be clearly labelled **with a blue triangle battery isolator sticker**. The master kill switch must be able to shut down the entire primary

electrical system for the vehicle. **The master kill switch must shut down the engine and fuel pump** when in the off position. Winch power supply and low amp draw secondary electrical equipment which requires an uninterrupted power supply may circumvent this switch. It is highly recommended that heavy-duty marine-style battery disconnect switches, capable of carrying total vehicle current load (including winch) be used and wired so that the entire electrical system can be disabled with one switch. THIS RULE HAS NO EXEMPTION FOR ALL CLASSES OF VEHICLE. ALL VEHICLES MUST BE ABLE TO BE COMPLETELY SHUT DOWN WHEN THE SWITCH IS IN THE "OFF" POSITION. This switch must interrupt the power transfer from the alternator to the engine.

2. This external kill device may be in the following forms; A kill switch or via a pull cable actuator from the in-car master switch.

### Ignition

1. Each vehicle must have a positive action on/off ignition switch. The switch must be labelled



“ignition on/off” and be located within easy reach of the driver and from the outside of vehicle. All electric fuel pumps with independent switches must be labelled “fuel on/off” and be within easy reach of the driver and navigator. It is highly recommended that electric fuel pumps not be independently switched.

### Batteries

1. Batteries must be securely mounted with metal brackets, clamps, or tie-downs in a manner that prevents displacement in a roll over. All flooded cell batteries mounted in the occupant’s compartment must be fully enclosed in a battery box, including the top, sides, and bottom. The enclosure must be able to contain the quantity of acid contained in the battery. Batteries shall not be located in the occupants’ compartment without fully enclosed boxes and vents outside of the occupants’ compartment. Batteries shall be considered as being in the occupant’s compartment if there is no firewall between the battery and the occupants. All batteries shall be the sealed, non-spill type. Absorbed glass mat (AGM) or “gel cell” type batteries are highly recommended.

## Lights (See Additional Class Specific Rules)

1. **All vehicles must have at least one working front light on at all times.**
2. All production 4wd and modified 4wd vehicles must have a minimum of two tail-lights, two brake lights and one rearward facing amber light. Stock tail-lights, if so equipped, are permitted as long as they remain on whenever the vehicle's ignition is on. Rear facing Amber light must be permanently switched with the battery isolator to allow a visual indication to event staff whether a vehicle is live or not.
3. All rearward-facing lights must be protected against damage that may be caused by a rollover. Taillights must be at least 50mm in diameter, or meet with Ultra4 Australia approval, and must be mounted in such a manner as to be clearly visible from the rear of the vehicle. Rearward facing amber light, must illuminate with a brightness that is at least equivalent to a 40-watt 12V automotive lamp. LED lamps of appropriate brightness are permitted. The amber lens must be deep-coated amber in colour, no other colour is permitted. The amber light must be mounted with no obstructions (i.e. not mounted behind any translucent



object), from any position. The amber light must be placed so as not to impair the vision of another driver approaching from the rear. All rearward-facing lights must be connected to the ignition switch or directly to a main battery power switch, such that they remain on whenever the vehicle's ignition is on.

4. If during an event any required light fails to operate, the light must be fixed or replaced at the next available pit before the vehicle can continue in the event.
5. **All vehicles must be fitted with 2 brake lights that are activated when braking. These must be on the out most point of the vehicle either in the factory position or mounted on a rear bar.**
6. **All vehicles fitted with Amber lights. The amber light MUST be on when the vehicle Kill switch is in the on position to indicate to others that the vehicle battery system is live.**

## Starter

1. All vehicles must be self-starting by use of an onboard electric starter.

## Wheels & Tyres

1. All vehicles shall have exactly four wheels, each with exactly one tyre.
2. All factory-built tyres from any manufacturer are permitted.
3. Tyres shall be visually checked for condition and must not be considered obviously unsafe by the Chief Technical Inspector.
4. Tyre studs, screws or any other items added to the tyre are not permitted. Grooving, sipping or other modifications that involve removing material from the tyre are permitted.
5. Bead locked rims, either internal or external are allowed in all classes.



6. **The carrying of a spare in Ultra4 is not required but is highly recommended. Ultra4 recommends a spare of the same size as the 4 drive tyres be fitted.**
7. Spare tyres must be mounted safely on the vehicle so that the spare wheel does not obstruct the occupant's forward vision while driving and must also be securely mounted so that it does not become loose or dangerous during race conditions.

## Occupant Safety Equipment

1. One-piece fire-suits are mandatory. Two-piece suits are not permitted. Suits must cover from the neck to the ankles, to the wrists **and have a minimum standard of SFI 3.3A/1**. The suits must not have any holes, rips, or tears, nor be worn thin. Suits must also be free from any petroleum-based contaminants. All suits must be made from fire-resistant material with the manufacturer's fire resistant rating label attached. All additional stitching, patches, advertising, emblems and embroidery must be fire resistant.

2. Helmets must be approved by, and bear the sticker of, the following: AS 1698, Australian Standard; Snell SA 90, M90, SA95, SA 2010 and SA 2015 USA Standard; SF1 Spec. 31.1, 31.2, USA Standard; SIS 88.24.11(2), Swedish Standard; DS 2124.1, Danish Standard; SFS 3653, Finnish Standard; ONS/OMK, German Standard; NE 5 72 305, French Standard; E22 (with 02, 03, 04 or 05 amendments), European Standard; BS 6658-85 Type A and A/FR (including amendments), British Standard as a minimum (Helmets exceeding these standards are accepted.).
3. Primary helmet fastening must be by means of straps using D-ring buckle. No snaps or Velcro will be permitted as the primary means of securing the helmet. Snaps or Velcro may be present as a means of securing the loose ends of the helmet's straps. The interior and exterior of the helmet must be free from defects (i.e., the padding must be in good condition and the exterior of the helmet must not be damaged). Ultra4 Australia strongly recommends that entrants use helmets specifically designed for motor racing.
4. Shatter resistant eye protection is required for all competitors in a Ultra4 Australia event –



whether that be by means of helmet visor or suitable goggles.

5. Neck braces or HANS devices are required for all competitors. Neck devices should provide adequate support and have a fire-resistant covering in good or like-new condition. Neck support must be made by a recognised manufacturer.
6. Footwear must be race specific, fire retardant boots. These must have a minimum rating of SFI 3.3A/5 or better.
7. All occupants of the vehicle must wear race specific fire-retardant gloves during racing. These gloves must have a minimum rating of SFI 3.3A/5 or better.
8. Gloves must be worn by all competitors when external from the vehicle under race conditions.

## Vehicle Weight

1. In future Ultra4 Australia events, vehicle weight may be recorded for handicap and development purposes.

## Stock Class

The spirit of the stock class is to allow lower budget racing as well as the ability for Australia's 4wd vendors to showcase their products in an off road environment. The Nominated Series Driver will bear the burden of proving legality of any part of their vehicle including but not limited to; Motor/Transmission, frame length, suspension configuration.

### Stock Class: Eligible Vehicles

1. Any and all four-wheeled, four-wheel-drive production vehicles are eligible for competition, providing they meet all the rules and regulations specified herein, and with the following limitations and exceptions: Minimum of one thousand (1000) units were produced by the original manufacturer for given model year, for given market/region.



### Stock Class: Frame & Body

1. Stock frame (frame is considered to be the primary frame-rails and all factory cross members) must be retained, and must be complete and unmodified. No material may be removed for any reason and no section of the frame may be 'massaged' or re-shaped with the following limitations and exceptions: The rear portion of the frame and rear crossmember may be removed or trimmed for the sole purpose installing an aftermarket rear bumper. Frames may be reinforced by adding material.
2. Stock body (body is considered to be the full cab, including all interior and exterior sheet metal, bed, doors, bonnet, fenders, grill, etc.) required. Must be complete and unmodified, with the following limitations and exceptions: Holes may be cut in any part of the body for the single and exclusive purpose of allowing roll cage tubes and transmission/transfer case linkage to pass-through the body. Open holes must be kept to within 25mm of the diameter of any tube or linkage that passes through the body.

3. Stock side windows (glass) are not permitted to be wound up during racing. Window nets shall be used for driver and co-driver side openings according the general vehicle specifications.
4. Front inner fenders must be complete and unmodified, with the following limitations and exceptions: outer fenders (wheel well openings) may be trimmed for the single and exclusive purpose of allowing for tyre clearance. Modifications to the outer fenders must preserve the look of the stock wheel wells, as originally manufactured, and must not be trimmed excessively (no more than a 50mm gap between any part of the outer fender and the tyre at full compression). Aftermarket flares will be allowed, providing they do not extend past the original body panels more than 50mm.
5. Front outer fenders may be replaced with OEM-style aftermarket fenders (flared fibreglass fenders are permitted).
6. Rear inner and outer fenders must be complete and unmodified, with the following



limitations and exceptions: outer fenders (wheel well openings) may be trimmed for the single and exclusive purpose of allowing for tyre clearance. Modifications to the outer fenders must preserve the look of the stock wheel wells, as originally manufactured, and must not be trimmed excessively (no more than a 50mm gap between any part of the outer fender and the tyre at full compression).

7. Extensive damage to any portion of the frame or body (prior to race start) may be considered illegal modifications, and repairs may be required, as determined by, and at the sole discretion of the Chief Technical Inspector.
8. Stock body mounts may be modified or eliminated to gain a maximum of 50mm lift vertically or, with the following limitations and exceptions: The relationship of the body to the frame must remain within 25mm of stock configuration (horizontally), as originally manufactured.
9. Factory head/taillights required and must be functional.

10. Factory bumpers are not required and may be modified or eliminated.

#### Stock Class: Engine

1. Any and all engines are permitted, providing they meet all the additional rules and regulations specified herein, and with the following limitations and exceptions: The original vehicle fire wall must not be cut, altered or moved from the factory location, no excessive holes are to be cut into the original firewall and the engine must remain within 200mm of the stock location.
2. Maximum engine capacity for all stock class vehicles will be 4.8ltrs with no exceptions. Engines may have forced induction.

#### Stock Class: Transmission

1. Any and all transmissions are permitted, providing they meet all the additional rules and regulations specified herein.



#### Stock Class: Transfer-Case

1. Any and all transfer-cases are permitted, providing they meet all the additional rules and regulations specified herein.

#### Stock Class: Driveshafts

1. Any and all driveshafts are permitted, providing they meet all the additional rules and regulations specified herein

#### Stock Class: Axles

1. Stock axle housings must be maintained but internal strengthening/ratio modifications may be made, providing they meet all the additional rules and regulations specified herein. Diff locks are allowed and recommended.

#### Stock Class: Steering

1. Steering components may be modified or eliminated and steering components/linkage may be installed in any location and orientation, with the following limitations and exceptions: All vehicles must retain some type of mechanical steering linkage (e. g. 'full-hydro' steering is



not permitted, unless factory-equipped), and said linkage must be capable of controlling the direction of the steering wheels/tyres without the benefit of any additional power-steering aids. The steering box (or rack, if so-equipped) must remain within 100mm of the stock location. Rear-steer is not permitted.

### Stock Class: Suspension

1. Wheelbase must remain within 3" of stock, as originally manufactured.
2. Suspension configuration must remain stock, as originally manufactured (meaning that leaf springs must remain leaf springs, coil springs must remain as coil springs, torsion-bars must remain as torsion-bars, etc.).
3. Leaf-springs may be replaced with any leaf springs and may be installed in any location and orientation, with the following limitations and exceptions: Leaf springs must be connected directly to the axle assembly, unless otherwise factory-equipped. Links/linkage may be installed, but the leaf springs must be capable of locating the axle assembly relative to the



4. Coil-springs and related suspension linkage may be modified or eliminated and replaced with any coil-springs and linkage, and may be installed in any location and orientation, with the following limitations and exceptions: Coil-springs must be connected directly to the axle assembly and chassis and shall not be mounted in any way so as to yield any type of mechanical advantage, unless factory-equipped. Coil-springs shall not be replaced with any type of coil-over shocks, unless factory-equipped.
5. Secondary suspension is not permitted, unless factory-equipped (secondary suspension is considered to be any means or method of supporting any portion of a vehicle's weight). As such, springs of all types, airbags, air-shocks and hydraulic/pneumatic rams are not allowed. Compressible bump stops made of rubber, foam, or other similar materials are permitted as well as air/nitrogen hydraulic bump stops with the following limitations and exceptions: Bump stops shall not have any effect whatsoever on any aspect of a vehicle's performance outside of the last 50mm of vertical wheel-travel (on compression).

6. Shock absorbers of any make/model/type are permitted and may be installed in any location and orientation, with the following limitations and exceptions: Only one shock absorber is permitted per wheel/tyre (not including spare tyres). Shock absorbers must be connected directly to the axle assembly and chassis and shall not be mounted in any way so as to yield any type of mechanical advantage, unless factory-equipped (mounting shock absorbers off-vertical is permitted and shall not be considered a mechanical advantage).

### Stock Class: Wheels & Tyres

1. Tyres must be DOT-approved, with a maximum outside diameter of 33.5" (or metric equivalent), as specified on the tyre's sidewall by the original manufacturer, unless otherwise stated in supplementary regulations covering special events.
2. Beadlocks are allowed.
3. Wheel spacers shall be allowed, providing the tyre does not extend past the guard more than



50mm.

### Modified Class

#### Modified Class: Chassis & Body

1. Stock chassis must be retained (chassis frame is considered to be the primary original manufacturers chassis rails). Original manufactures chassis rails must remain from 300mm in front of the original firewall or the front most engine-mounts whichever is greater and can finish behind the "B" pillar on a 2 door vehicle and "C" pillar on a 4 door vehicle. The balance of the frame should remain whole including original chassis manufacturer's width and height.
2. Stock body (body is considered to be the exterior of the cab, bed, doors, hood, fenders, grill, etc.) must be complete, with the following limitations and exceptions: Modifications to the body for performance and/or clearance are permitted, but must preserve the look of the stock body, as originally manufactured (60% front profile, 50% side profile).

### Modified Class: Engine

1. Any and all engines are permitted, providing they meet all the additional rules and regulations specified herein, and with the following limitations and exceptions: The back of the engine-block must be located in front of the furthest-forward portion of the driver's seat, unless otherwise-equipped, as originally manufactured.
2. Radiator mounting open

### Modified Class: Transmission

1. Any and all transmissions are permitted, providing they meet all the additional rules and regulations specified herein.

### Modified Class: Transfer-Case

1. Any and all transfer-cases are permitted, providing they meet all the additional rules and regulations specified herein.



### Modified Class: Driveshafts

1. Any and all driveshafts are permitted, providing they meet all the additional rules and regulations specified herein.

### Modified Class: Axles

1. Any and all axles are permitted, providing they meet all the additional rules and regulations specified herein.

### Modified Class: Steering

1. Any and all steering assemblies are permitted, providing they meet all the additional rules and regulations specified herein. Rear-steer IS permitted.

### Modified Class: Suspension

1. Any and all suspension components and configurations are permitted, providing they meet all the additional rules and regulations specified herein.

2. Shock absorbers of any make/model/type are permitted and may be installed in any location and orientation, with the following limitations and exceptions: Only two shock absorbers are permitted per wheel/tyre (not including spare tyres).
3. Manual suspension controls (e.g. forced hydraulics/pneumatic) are not permitted.

### Modified Class: Wheels & Tyres

1. Tyres can have a maximum outside diameter of 37.5" (or equivalent), as specified on the tyre's sidewall by the original manufacturer, unless otherwise stated in supplementary regulations covering special events.
2. Beadlocked rims are allowed.

## UTV Class

### UTV Class: Frame & Body

1. All UTV vehicles must utilise the stock chassis and maintain stock appearance. The stock



chassis is defined as the main lower rails running along the inner sides of the UTV and the front and rear tubes that connect them. Any modifications to the stock chassis must be pre-approved by a Ultra4 Australia tech inspector. The stock chassis may be added to, for durability and strength, but must retain the stock width, length, and configuration. The stock UTV cage may only be used upon prior approval and with sufficient bracing. Cage design must otherwise meet the minimum requirements herein the Ultra4 Australia Rulebook. All body parts must remain on the vehicle (accidental damage excluded) during the entire length of race. The roof must be covered with sheet metal or aluminium as per the general vehicle specifications. Frame number must remain standard to original purchase. Frame geometry must remain as designed by the OEM.

2. **Each side of the automobile next to the occupants shall be fitted with a panel of sufficiently rigid material up to the height of the lower edge of the scuttle panel. This shall provide protection to the occupants from the ingress of material thrown from the wheels, and assist to restrain the occupants' limbs within the confines of the automobile structure.**

3. UTV width can be a maximum (measured outside of tyre to outside of tyre) of 74 inches (1.9 meters).
4. General entrant vehicle rules apply to this class including safety, lighting, occupant safety, kill switches and roll over protection.

#### UTV Class: Engine

1. Any and all engines are permitted, providing they meet all the additional rules and regulations specified herein, and with the following limitations and exceptions: The engine must remain as a factory option from the make specific OEM.
2. Maximum engine displacement is 1100cc.
3. Aftermarket Nitrous Oxide and forced induction is allowed, providing they meet all the additional rules and regulations specified herein.



4. Radiator mounting open

#### UTV Class: Transmission

1. Every vehicle must use the stock transmission and clutch design. Every vehicle must have a functional reverse gear.

#### UTV Class: Driveshafts

1. Any and all driveshafts are permitted, providing they meet all the additional rules and regulations specified herein.

#### UTV Class: Steering

1. Any and all steering assemblies are permitted, providing they meet all the additional rules and regulations specified herein.

#### UTV Class: Suspension

1. All a-arm mounting points must remain the stock design and remain in the stock location as delivered from the manufacturer, however they may be reinforced for strength.

2. Shock absorbers of any make/model/type are permitted and may be installed in any location and orientation.
3. Any bump stop is allowed.
4. Manual suspension controls (e.g. forced hydraulics) are not permitted.
5. The maximum overall wheelbase from spindle to spindle can be increased by 8 inches over standard.

#### UTV Class: Wheels & Tyres

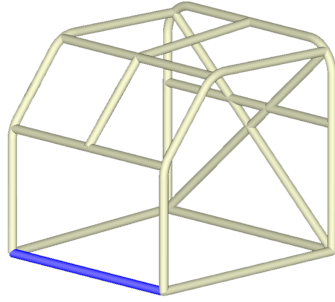
1. **There is no limits on wheels and tyres**

### Ultra 4 & Ultra4 Limited

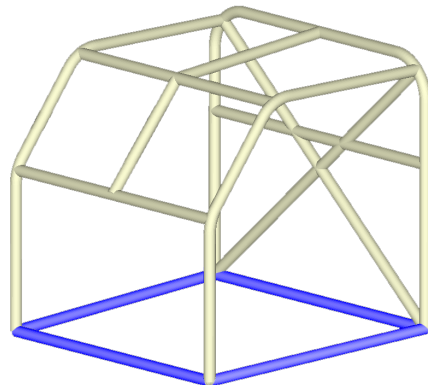
#### Ultra4 & Ultra4 Limited: Frame & Body



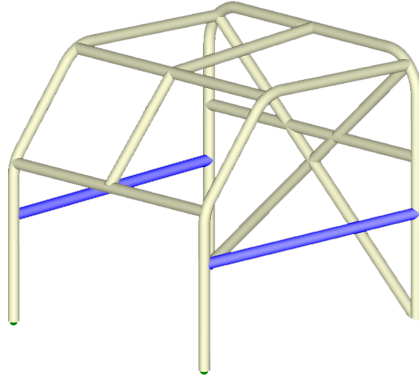
1. Any and all frame and body modifications are permitted, providing they meet all the additional rules and regulations specified herein.
2. Frames may be 100% tube orientated, with no real resemblance to any make or model of vehicle. All chassis/frames must meet the additional rules and regulations specified herein.
3. A Front cross member must connect the feet of both left and right-hand side A pillar feet. This cross member may have either a horse shoe design to clear driveline components or with pre-approval/scrutineering, have the centre omitted.



4. Lower side bars must connect the bottom of the A pillar and the B pillars in both directions. From this lower sub frame, all occupant's seats and harness points must be fabricated, leaving occupants in a secure safety cell.



5. Both Ultra4 classed vehicles are required to have additional side intrusion bars.



#### Ultra4 & Ultra4 Limited: Engine – Ultra4

1. Any and all engines are permitted, providing they meet all the additional rules and



regulations specified herein.

#### Ultra4 & Ultra4 Limited: Engine - Ultra4 Limited Class

1. Naturally aspirated engine size shall be no greater than 3.9 litres **V configuration** or **4.0 litres straight configuration**. Forced induction engines shall be no greater in size then 2.0 litres. No boring or stroking of original engine displacement.

#### Ultra4 & Ultra4 Limited: Transfer-Case

1. Any and all transfer-cases are permitted, providing they meet all the additional rules and regulations specified herein.

#### Ultra4 & Ultra4 Limited: Driveshafts

1. Any and all driveshafts are permitted, providing they meet all the additional rules and regulations specified herein.



### Ultra4 & Ultra4 Limited: Axles

1. Any and all axle assemblies are permitted, providing they meet all the additional rules and regulations specified herein.

### Ultra4 & Ultra4 Limited: Steering

1. Any and all steering components and configurations are permitted, providing they meet all the additional rules and regulations specified herein. Rear steering is permitted. Any steering wheels that have a quick release mechanism must be in a good serviceable order and will be tested prior to the event.

### Ultra4 & Ultra4 Limited: Suspension

1. Any and all suspension components and configurations are permitted, providing they meet all the additional rules and regulations specified herein.
2. Shock absorbers of any make/model/type are permitted and may be installed in any location and orientation, providing they meet all the additional rules and regulations specified herein.



3. Manual suspension controls (e.g. forced hydraulics) are permitted.

### Ultra4 & Ultra4 Limited: Wheels & Tyres

1. Any and all tyres are permitted, providing they meet all the additional rules and regulations.

### Ultra4 & Ultra4 Limited: Lights

1. Working headlights are only required for events where any portion of the on-course event takes place between sunset and sunrise.
2. All vehicles must have a minimum of two red brake lights and one amber battery isolator light.
3. **All Ultra4 classed vehicles must have at least one blue flashing rear mounted dust light that is active only when the ignition is turned on.**

4. These lights must be mounted so that the two brake lights are to the outside edge of light mount and the amber light to the left of the centreline of the vehicle.
5. All rearward-facing lights must be protected against damage that may be caused by a rollover. Rearward facing amber light, and blue light if so required, must illuminate with a brightness that is at least equivalent to a 40-watt 12V automotive lamp but not brighter than equivalent to a 55 watt 12V automotive lamp. LED lamps of appropriate brightness are permitted. The amber light must be placed so as not to impair the vision of another driver approaching from the rear. The brake lights must only work when the vehicle is braking, the amber light must only work whenever the battery isolator is.
6. If during an event any required light fails to operate, the light must be fixed or replaced at the next available pit before the vehicle can continue in the event.

## General Event Rules and Regulations

The following event rules are general rules for all Ultra4 events. Event specific rules will apply to all



events held by Ultra4 Australia. These rules will be available when you register for that event.

## Technical Rules and Regulations

1. It is the Nominated Series Driver's responsibility to ensure their vehicle meets and/or abides by all Ultra4 Australia technical rules, regulations, and specifications prior to and during an event.
2. Ultra4 Australia reserves the right to limit the number of personnel allowed into any area or garage in which inspections are being made or within which vehicles are impounded.
3. Ultra4 Australia reserves the right to seal or impound any entrant vehicle.
4. The Directors, Race Director and/or Chief Technical Inspector may impound any vehicle or vehicle parts.

5. Ultra4 Australia assumes no responsibility for impounded vehicles. Ultra4 Australia intends to make reasonable efforts to ensure the security of impounded vehicles.
6. The Nominated Team Manager shall be responsible for providing the Chief Technical Inspector with documentation and records pertaining to compliance of any and all rules specified herein.
7. Any/all vehicles maybe subject to a post-race impound and a second more thorough tech inspection.
8. No vehicle shall be removed from an inspection area or impound area without permission from the Directors, Race Director or Chief Technical Inspector. Any vehicle removed without appropriate permission shall subject that entrant to disqualification. Any vehicle not taken directly to the inspection or impound area when requested by the Race Director or Chief Technical Inspector shall subject that entrant to disqualification.



9. The Chief Technical Inspector may seize any illegal parts or devices found on any vehicle. Any item seized by the Chief Technical Inspector will not be returned until after the event and any CRC hearing. Ultra4 Australia assumes no responsibility for seized parts or devices. Ultra4 Australia intends to make reasonable efforts to ensure the security of impounded vehicles/parts/devices.
10. Failure of entrants to present themselves at registration and pre-event technical inspection/contingency lineup during the hours announced or listed on event information sheets may result in the following penalties, at the discretion of Ultra4 Australia
11. Failure to show up for final registration call: DNS
12. Failure to show up on time for technical inspection:
  - First offence: One hundred dollar (\$100) fine to be paid before technical inspection.

- Second offence: One hundred dollar (\$100) fine and a 30-second time penalty per 10km of course (i.e., 80km course will result in a 4-minute time penalty).
  - Third and subsequent offences: One hundred and fifty dollar (\$150) fine and a 1-minute time penalty per 10km of course (i.e., 100km course will result in a 10-minute time penalty)
13. Pre-race technical inspection will take place at the end of the contingency row display. Each car must travel through contingency before technical inspection. Any mandated tracking devices must be in place before entering technical inspection area.
14. The personal protective gear of all competitors will be checked at pre-race technical inspection. This includes, but is not limited to, fire-suits, helmets and neck braces. First-aid kits, fire extinguishers, seat belts, and nets will also be checked. This does not imply that these items will be the only items checked. The Chief Technical Inspector or assistant Chief Technical Inspector may seize any personal protective gear that does not comply with the rules or is deemed unsafe. Any item seized by the Chief Technical Inspector will not be



returned until after the event and any CRC hearing. Ultra4 Australia assumes no responsibility for seized parts or devices. Ultra4 Australia intends to make reasonable efforts to ensure the security of impounded vehicles/parts/devices.

15. Pre-race impound will be at the discretion of Ultra4 Australia After technical inspection, vehicles will be directed to an impound area where they will remain until assigned removal time. Only designated Ultra4 Australia officials will be permitted into the impound area after a vehicle is placed in impound. All other personnel must receive special written permission from Ultra4 Australia to enter the impound area after vehicle is placed in impound.
16. Ultra4 Australia reserves the right to subject any vehicle to a technical inspection after the event, at the discretion of the Race Director and/or Chief Technical Inspector. In a post-race technical inspection, the Nominated Series Driver will be responsible for removing or having removed or preparing the requested items to be inspected, as directed. Failure to comply will result in disqualification of entrant and may result in suspension.

17. The CRC or Directors may require an entrant vehicle damaged in an event-related incident to submit to post-incident inspection. If the owner or driver refuses to allow the vehicle to be inspected, the vehicle and driver may be disqualified and suspended from future Ultra4 Australia events.
18. Post-race impound of all finishing vehicles will be at the discretion of Ultra4 Australia. If impounded post-race, vehicles will be released no later than two hours after the official finish of the event. Vehicles involved in any type of protest or complaint will be held in impound until after the CRC rules on the protest or complaint.
19. Any refusal by a competitor to comply with CRC rulings shall result in competitor's disqualification and suspension of competitor from all Ultra4 Australia sponsored events for a period of not less than one year.
20. During racing at the event it is the sole responsibility of the team manager to make sure that the vehicle pre check list is filled out and signed off by the team manager and that the



vehicle is in race condition prior to starting every lap as per Ultra4 series pre-check sheet.

Ultra4 Australia takes no responsibility for unsafe vehicles on the track that have been signed off by their team manager.

### Passing On The Course Under Racing Environments

1. All classes are to adhere to these guidelines and regulations as to safely passing slower vehicles whether in the same class or a lower class.
2. All passing is to be done in a safe manner and any entrant placing any one else in undue danger shall be dealt with but the CRC and possibly disqualified or reprimanded.
3. Passing shall be done in the following manner:
  - On catching up to a slower entrant, you shall provide one (1) two second application of your vehicles siren, allowing sufficient track time for the slower vehicle to heed to your warning and pull aside.

- If the slower vehicle does not heed to your first warning, sound your siren again, for three, one (1) second bursts, once again allowing sufficient track safety and time for the slower vehicle to pull over.
  - If the slower vehicle still does not heed to your warning, then assess the situation again. You are well within your rights to then report this entrants unsportsmanlike conduct in slowing you down.
4. Ultra4 Australia does not condone physical contact of vehicles, should you make contact with the slower vehicle, this shall be deemed as allowed according to the Ultra4 Australia Rulebook.

### Nerfing – Tapping Vehicles During An Event

1. Deliberate contact between vehicles is discouraged upon at all Ultra4 Australia Events.
2. Stock Class Vehicles will not, under any circumstances be allowed to make deliberate contact with another entrant. This action will result in penalties and or disqualification.



3. Nerfing in Modified and Side by Side classes will be allowed but there will be rules and regulations herein that stipulate the ability to do so.
4. Nerfing in Ultra4 class will be seen as acceptable for ALL entrants. It is expected that you build or modify your vehicle to be suitable for this style of racing.

## Event

### Registration

1. An entrant number shall be assigned to the Nominated Series Driver for the entire year. The points stay with the Nominated Series Driver and the entrant number for the entire year. Nominated Series Driver must be a registered competitor in every event and must start or finish every event in a competing race vehicle bearing his/her assigned entrant number. Nominated Series Driver must also, for every event, have signed all releases required by Ultra4 Australia

2. Nominated Series Driver and assigned entrant number form an inseparable pair for each year. No Nominated Series Driver may switch assigned entrant numbers during the year or be the registered Nominated Series Driver for more than one entrant number; and no entrant number may have more than one registered Nominated Series Driver during the year. The actual event vehicle may be modified, upgraded, or replaced (subject to certain series restrictions), but the Nominated Series Driver and his/her assigned entrant number may not be altered, modified, swapped, traded, bought, sold, or otherwise changed. Only on special request and acceptance from the Production Directors may another driver step in for a Nominated Series Driver. This is up to the discretion of the Production Directors and may result in non compliance with the race series.
3. Any participant who fails to complete and sign the required entry forms and releases will be subject to disqualification. Releases must be signed in person, witnessed by Ultra4 Australia entry personnel. Government issued photo identification may be required.



4. All competitors under 18 years of age at the start of the event must have their entry form notarized and their release signed by a parent or legal guardian. No person under 18 years of age at the start of the event shall be the Nominated Series Driver for any Ultra4 Australia event. Team managers must be over the age of 18.
5. Nominated Series Driver and/or co-driver(s) who are listed on the official Ultra4 Australia entry form must attend all drivers' meetings. Failure to do so may result in penalisation, denial of right to start, disqualification, and/or fines. Armband checks and written roll calls may be made at the meeting.
6. No participant may enter racing areas, pre-run, or receive the official course map until they have signed all entry forms and releases. No person shall sign any entry form or release for any other person.
7. Special consideration registration may be permitted with advanced approval of Ultra4 Australia

8. All teams/entrants shall have a Nominated Team Manager. This team manager shall be responsible for making sure the team is at all the official meetings, briefings, start times and any other times or events sanctioned by the Race Directors. This Team manager shall be the sole communicator between race officials pre and post event unless there is a CRC inquiry. The nominated team manager shall handle all pre race/lap checklists and talk directly to the event officials.

## Conduct

1. Any competitor having an application bearing a falsified signature shall be disqualified. Competitor may also be suspended from future events for one year.
2. Failure to attend the drivers' meeting by at least one member of entrant: penalisation, denial of right to start, disqualification and/or fines.
3. Abusive conduct toward a race official: Disqualification, suspension, minimum two hundred



dollar (\$200) fine (taken off any prize winnings – per round) or any combination of the three.

4. Drinking intoxicating beverages in the official pre-race technical inspection area, post race areas (e.g. impound, etc.), pits, on the racecourse or in the surrounding areas before, during or before final presentation by any person is strictly forbidden. The use of narcotics or other illegal or illicit drugs is forbidden. Any participant that shows any evidence whatsoever of being under the influence of any of the aforementioned may be immediately disqualified and subject to suspension from all future Ultra4 Australia events. This includes ALL pit crew and/or anyone holding a pass into the pit/competitor area. Violator must leave the premises immediately at the direction of the Directors.
5. Any participant who subjects any Ultra4 Australia official, event staff, volunteer, other event participant or spectator to improper language, verbal threats and/or physical abuse, or any other offensive, harassing, or demeaning language or behaviour shall suffer automatic disqualification or be brought before the CRC. If brought before the CRC, the CRC will make the decision of disqualification and/or suspension of the offending competitor and/or



participant for said offences. Competitors are responsible for the behaviour of all participants acting on their behalf, including but not limited to: their pit crew, support crew, and sponsors. Acts of physical or verbal abuse may be reported to the proper authorities and may lead to legal action. Poor sportsmanship or unsportsmanlike conduct, in the pits or on the course, before, during, or after an official Ultra4 Australia event may subject offending or related entrants to disqualification.

6. Failure to appear before the CRC when requested may result in a letter of reprimand and/or penalty at the discretion of the CRC. Failure to appear before the CRC when requested twice within a race season may result in a penalty and/or suspension for up to one year at the discretion of the CRC.

### Drivers & Co-Drivers

1. Only competitors that are listed on the official Ultra4 Australia entry form may drive or co-drive the vehicle for which they are registered. Annual registration is limited to a maximum of three co-drivers per vehicle.



2. All competitors must be at least 16yrs old at the start of the event.
3. No person other than the registered competitors of an entrants vehicle shall ride in, on, or occupy that vehicle during an event. No competitor shall ride in or on their vehicle other than in the normal driving/riding positions. Enforcement of these rules for any entrant vehicle is the responsibility of the Nominated Series Driver for that vehicle. A maximum of 2 occupant per vehicle are allowed at any one time, regardless of seating capacity and any occupant must be registered at that event.
4. Competitors may exit vehicle during event as follows:
  - Co-driver(s) may exit vehicle on course to spot, winch, repair vehicle, or bio break.
  - Driver may exit vehicle on course to repair vehicle or bio break.
  - Competitors may exit vehicle in designated pits.
  - No person may exit vehicle at any time unless it is safe to do so.

- Competitors may not intentionally choose to exit vehicle for any reason if, by stopping the vehicle and doing so, they will impede the flow of traffic, providing the vehicle is capable of moving under its own power at the time.
5. No person may be registered as the Nominated Series Driver for more than one vehicle at the same event. The Nominated Series Driver may be registered in another vehicle as a co-driver.

### Event Course

1. Ultra4 Australia will set the maximum duration and length of an event.
2. An entrant's official time shall be the total elapsed time between their assigned starting time and the time they cross the official finish line. This elapsed time must be less than the designated time limit of the event. If an entrant does not report to the staging area in time to stage in their respective starting position, the entrant will be placed at the rear and will start after the last assigned start time for their vehicle class. If an entrant does not report to the



staging area in time to start before the last assigned starter leaves the starting line, they shall be judged DNS. In all cases, if the entrant does not start at their assigned time, their time will still start from their assigned starting time, not their actual starting time. Total time on course will remain the same for ALL entrants per class. This is where pre-qualifying for pole position will enable drivers more physical time to complete the course requirements.

3. The winner/s of the event, shall be the entrant that either: finishes the race with the lowest elapsed time, or completes the most laps or greatest distance within the event time limit, or receives the best lap score for the event. The entrant must also meet all other criteria and must not be disqualified in order to be declared the official event winner.
4. When encountering an on-course accident, rollover, breakdown, or disabled vehicle, all entrants must make reasonable efforts to assess the condition of the competitors involved. Competitors who are involved in an accident, rollover, breakdown, or whose vehicle is disabled must make every effort when OK, to signal their condition to passing competitors (e.g. give a thumbs up). If, upon encountering an on-course accident, rollover, breakdown, or

disabled vehicle, a passing vehicle is unable to determine that the competitors involved are OK or if there is any doubt as to the status or condition of the competitors involved, they must inform a Ultra4 Australia official at the next checkpoint, road crossing, or pit stop or by radio of the location, vehicle number, and any apparent injuries. All entrants must retrieve a stuck stub from any other entrant who is displaying the stuck stub and deliver the stuck stub to the next available race official. Failure to assist fellow competitors with passing on such a stuck stub may result in disqualification and/or reprimand.

5. ALL team members who are outside a vehicle on the event track are required to wear a form of high visibility clothing that has a form of reflective tape for night time events. This applies to competitors broken or DNF'd on the side of the track or offering external assistance to another competitor who is stuck on the track.
6. Breakdown safety devices (beacons, glow sticks, or reflective devices) must be placed at least 10 meters and again at approximately 30 meters behind any breakdown or accident and be placed beside the track on the same side of track as the disabled vehicle.



7. Any entrant who must discontinue the event must report, in person or via the stuck stub, to a race official at a checkpoint, pit stop or start/finish that they are out of the race.
8. No aircraft are permitted for the purpose of race support. This includes, but is not limited to, flying over any race vehicle; transportation of competitors and/or support crews (unless a medical emergency exists); communication with race vehicle; spotting for race vehicle; transportation of equipment and/or parts; landing on or near the race course in areas other than approved by Ultra4 Australia and within Australian Airspace rules; and interfering with the normal conduct of the event. Violation of this rule may lead to entrant's disqualification. Requests for aircraft special use (i.e. filming, observing, etc.) must be submitted to Ultra4 Australia in writing. Requests must include the radio frequencies (helicopter or aircraft frequency and race team frequency) to be used and must be submitted no later than one month before scheduled event. Any request received during the month prior to the event shall not be considered.

9. Starting procedures will be announced at the drivers' meeting at each event.
10. No entrant may leave the start line before their assigned start time. Only those entrants that cross the finish line within the designated time limit will be declared official finishers. Every vehicle must come to a complete stop at each checkpoint. Failing to come to a complete stop at any checkpoint will subject the offending entrant to a minimum fifteen minute time penalty for each occurrence and possibly disqualification, at the discretion of the CRC. Failing to do so will subject the offending entrant to a minimum five minute time penalty for each occurrence and possibly disqualification, at the discretion of the CRC.
11. No entrant vehicle shall be towed, pushed, pulled or otherwise moved or transported by any non-entrant vehicle, or spectator group on the official course while an event is still in progress. Another race-entered vehicle or an official Ultra4 Australia vehicle may push, pull or tow the race-entered vehicle out of the flow of traffic but may not push, pull or tow it through the race course, a pit stop or a checkpoint. Occupants of a vehicle that is pushed pulled or towed clear of traffic must make necessary repairs to leave, under their own power,



the area to which they were relocated. No vehicle may be pushed, pulled or towed by another vehicle within the last one hundred meters (100mtrs) of the finish line.

12. A marked course is that official route designated by and marked with official Ultra4 Australia markings and/or indicated via the official GPS track. All vehicles must follow this route during the event. Passing is only permitted where there is no vegetation on the side of the course. Short coursing is not permitted and shall result in disqualification. Short coursing is defined as any deviation from the marked course for any reason. No deviation from the marked course, including passing, is permitted. Deviation from the marked course shall result in automatic disqualification. Sensitive areas shall be marked by Ultra4 Australia markings and signs. Ultra4 Australia is not responsible for markings that are damaged or removed. All vehicles shall drive only in the correct direction of the course route. Driving in the opposite direction of the course is prohibited and shall be grounds for penalties of up to and including disqualification and suspension.

13. Failure to stage or report to starting line-up by posted or announced time; or failure to stage or line up in time to start at assigned starting time: Rear start or DNS.
14. Failure to come to a complete stop at any road crossing designated as a full-stop crossing, or failing to stop when signalled to do so by road crossing officials: Fifteen-minute time penalty for each occurrence.
15. Speeding in a restricted speed area up to 10 km/h over announced or posted speed limit by race vehicle or support vehicles: One position penalty.
16. Speeding in a restricted speed area in excess of 10 km/h over announced or posted speed limit by race vehicle or support vehicles: Disqualification.
17. Short coursing: Disqualification.



18. Excessive or abusive nerfing or bumping: Disqualification.
19. Any competitor or competitor's crew member traveling on the course before the official event finish in other than event-registered vehicles participating in the event, may subject entrant to penalties of up to and including disqualification and suspension. There is no outside assistance permitted on the course or near the course during the event except for in the official areas designated for pits and/or fuel stops. Ultra4 Australia reserves the right to assess each situation and respond accordingly. Situations involving safety are at the discretion of the Directors. If a vehicle breaks down on the course, or rolls over and requires assistance to be righted, Ultra4 Australia approves the following options. Any deviation from these options may result in entrant being disqualified.
20. Driver or Co-driver who is with vehicle at time of breakdown may travel by foot to and from an official Ultra4 Australia designated pit stop of their choice, in order to retrieve equipment or parts necessary to repair vehicle. Except as detailed below in 19.5.21, if any other person delivers equipment or parts to entrant vehicle, entrant will be subject to disqualification.

Obtaining equipment or parts from any location other than an official Ultra4 Australia designated pit stop will subject entrant to disqualification.

21. Another race-entered vehicle may pick up equipment or parts at an official Ultra4 Australia designated pit stop (but not from any other location), and then deliver that equipment and/or those parts to broken down vehicle. The race vehicle picking up and delivering the equipment or parts must travel in the proper direction on the course. Traveling the wrong way on the course will subject both entrants (broken down vehicle and delivery vehicle) to disqualification. If any pit support vehicle or other vehicle not registered and competing in the event, delivers anything to a broken down vehicle, that broken down vehicle entrant shall be subject to disqualification. No entrant that has officially finished the event or has ceased competing may re-enter the course to deliver anything to a broken down or stranded vehicle. With the permission of the SRD or Race Director, DNF vehicles or vehicles having officially finished the event may enter the course after the close of the course checkpoint immediately after a broken or stranded vehicle to retrieve the broken or stranded vehicle and return it to main pit as a DNF, providing that in doing so neither vehicle travels or crosses any part of the



22. To minimise environmental impact and minimise traffic congestion on the course, Ultra4 Australia will provide official recovery crews to aid in the righting of vehicles that have rolled over. Entrants may use this service, if available, or other outside assistance (i.e. assistance from spectators) ONLY to right a rolled vehicle and in the presence of an Ultra4 Australia official. It is up to the the Nominated Series Driver to police this and not allow any outside assistance. Once righted, vehicle must not receive any further outside assistance of any kind. If able to continue after having been righted, vehicle may continue on course without penalty. If vehicle cannot continue after righting but does not, in the opinion of the officials, pose an impediment to traffic flow, entrants may attempt to repair vehicle and continue, but may not receive any outside assistance in doing so. If a righted vehicle cannot continue and is, in the opinion of the officials, an impediment to traffic flow, a DNF will apply.
23. Any entrant vehicle that is unable to continue, whether or not it has been righted with or without outside assistance, and that as a result of being disabled poses, in the opinion of the

officials, an impediment to traffic flow, may be relocated on, or removed from, the course by means of official recovery crews or other outside assistance only to the point that they no longer pose an impediment to traffic flow. Once so relocated or removed, competitors may attempt to repair vehicle and continue, but may not receive any outside assistance in doing so.

24. Competitors may not receive any help in repairing a damaged vehicle outside approved pit areas. At no time may a team accept outside assistance from spectators to navigate an obstacle. (e.g. verbal and/or physical assistance.) At no time, regardless of circumstance, shall an entrant receive outside assistance which unfairly benefits that entrant. Receiving advice, guidance, or spotting through an obstacle from anyone other than a co-entrant or co-competitor may be considered an act of outside assistance.
25. No participant or any other person except a Ultra4 Australia official shall remove, alter, or relocate course markings. Any person found to have removed, altered or relocated course markings will be subject to disqualification and/or immediate removal from the event area



and may be banned from future Ultra4 Australia events.

26. Pre-running and course knowledge obtained thereby is the responsibility of every entrant of a Ultra4 Australia event. Pre-running must be done in a safe and sensible manner and will be restricted to only be run during designated pre-running time allocations, under the supervision of a Ultra4 Australia official vehicle. Unsafe and/or irresponsible driving during pre-running will subject entrants to penalties of up to and including disqualification and suspension. Participation in pre-running is at entrants' own risk, and may not be part of the official event. All pre-running should be conducted according to park or land-use rules. Pre-runners must be aware of, and abide by, any rules or regulations regarding the use of public or park property. Participants engaged in pre-running shall always consider safety as the highest priority and be aware of other recreational users of the area in which they are pre-running in the event that the location is yet to be locked down in race mode.

## Checkpoints & Road-Crossings

1. In the event that a vehicle, be required to stop at a road crossing or checkpoint, all vehicles must enter each checkpoint or designated road crossing at a safe speed. Unsafe racing into and/or through any checkpoint or designated road crossing is prohibited. Speeding through a checkpoint or designated road crossing shall result in disqualification. Safe speed is defined as a speed at which a vehicle can make a controlled stop without endangering anyone in the immediate vicinity of the checkpoint or designated road crossing.
2. Passing is not permitted within 100 meters on either side of any road crossing or checkpoint, except at the direction of a Ultra4 Australia official. Failure to comply shall subject entrant to disqualification or a time penalty of a minimum of 10 minutes for each occurrence, at the discretion of the CRC.
3. Rolling through a checkpoint (i.e. failure to come to a complete stop): 10-minute time penalty for each occurrence.



4. Speeding through and/or unsafe racing up to or through a checkpoint: Disqualification.

## Pits

1. Stationary pits or chase crews outside of designated areas or traveling in restricted areas  
Race Directors or Senior Track Official: Disqualification.
2. Reckless driving in pit areas or on any access roads by race vehicle or support vehicles:  
Disqualification.
3. No vehicle shall be permitted to enter pit areas or course area without a valid Ultra4 Australia pit pass, displayed as prescribed in this rulebook.
4. At all times the Nominated Series Driver assumes responsibility for the actions of his/her pit crews, support crews, and all others associated with his/her team.



5. No person under suspension by Ultra4 Australia will be permitted to participate in any event or be permitted to enter the pits or course area.
6. Any pit support vehicle running on or near the course shall result in the entrant being disqualified. Any pit support vehicle traveling in a restricted area shall result in entrant being disqualified. Any pit support vehicle stopping to offer assistance at a road crossing may cause entrant to be disqualified. Any pit support vehicle stopping on a road that is near the course and not in an official, designated area may cause entrant to be disqualified.
7. Pits (stationary, roving or chase) located in any areas other than those areas officially designated as pit areas by Ultra4 Australia: Disqualification.
8. Any competitor, crew member, or other pit pass holder who takes part in any demonstration in the pits, on the course, or in the surrounding area before, during, or after an event shall be subject to expulsion from the area, suspension from future Ultra4 Australia events, and possible legal action.



9. Maximum speed limit on all main pit access roads and in all pit areas shall be 20 km/h for all vehicles. Maximum speed limit on all other access roads shall be 40 km/h Ultra4 Australia reserves the right to change speed limits to account for conditions.
10. The Ultra4 Australia Pit Stop Captain shall determine the pitting zone around any pit stop.
11. In the event that an Ultra4 Australia event have a remote pit stop, all pit supplies must be at least 10 meters from the edge of the course. No pit may be in the first 20 meters leading into, or the first 50 meters leading out of, a turn. Pits located within this turn area or less than 10 meters from the course will subject entrant to penalties of up to and including a minimum 20-minute time penalty or disqualification, at the discretion of the CRC.
12. All pits must have the equivalent of an approved 7kg ABC fire extinguisher at all times. This capability may be accomplished using fire extinguishers of any combination (minimum 0.9kg per extinguisher). These approved extinguishers must be supplied by the race crews.

13. All pit fire extinguishers must have current tag in place and be fully charged. Random checks will be made, and time penalties may be assessed if extinguishers do not meet these expectations.
14. All young children and pets must be kept out of the immediate area where vehicles will pit. Pets must be kept on a leash. All campfires must be kept out of immediate pit area. Campfires must not be placed between the course and pit. Campfires may not be permitted due to local fire bans. Firewood with nails is prohibited.
15. All entrants are responsible for cleaning the pit areas they used during the event.
16. All fuel cans must be kept off native soil and be stored on an impervious barrier. Vehicle refuelling shall take place over an approved fuel mat or tarp acting as an impervious barrier. Fuel mats or tarps shall be free from defects or tears which could result in a fluid spill onto native soil. Fluid absorption products are highly recommended. Teams responsible for fuel



spill onto native soil may be fined and liable for clean-up expense. Any team refuelling outside an approved pit location, or without a fuel mat or tarp, may face disqualification. This may seem over the top, but the Ultra4 Australia staff are dedicated to improving our sport's image and professionalism.

17. During refuelling, vehicles must be switched off and battery isolators turned onto the "off" position.
18. Refuelling in the pit area must take place within the teams nominated pit area or an area nominated by Ultra4 Australia. The vehicle being refuelled must be on a fuel mat or tarp.
19. The person refuelling the vehicle must be wearing the following PPE: Enclosed shoes/Boots, Long pants, Long sleeve shirt, Gloves and safety goggles.

20. In addition to the person refuelling the vehicle Ultra4 Australia require a fellow team member to be on standby with a minimum 7kg ABC fire extinguisher. No other person is to be standing within a 2-metre radius of the person refuelling.
21. All team pit areas, whether in a main pit lane or remote must have at a minimum of one (1) x 25 litre, brightly coloured garbage bag that, in the event of a liquid spill or environmental contamination, be used to remove contaminated soil from the location.
22. **During racing only pit crews that have signed the damage waiver and have a pit access arm band are allowed to work on any competition vehicle.**

## Communications

1. Ultra4 Australia uses the UHF Channel 13 as a main race channel for stand-alone events. Ultra4 Australia reserves the right to change the main race channel frequency if deemed necessary. Ultra4 Australia will make all reasonable efforts to notify all entrants of any changes to the main race channel frequency. Medical emergency operations shall use the



Channel 13 during all stand-alone Ultra4 Australia events. All radio or other transmissions which interfere with Ultra4 Australia control communications are strictly prohibited except in the case of medical emergencies. All race and support-vehicle radio equipment is strictly prohibited from interfering with or disrupting race communications on all frequencies allotted to the amateur radio band, public service band, citizens band, marine band and aircraft band as defined by local rules. Outboard linear amplifiers with an output exceeding 25 watts are prohibited. An outboard linear amplifier is a device that boosts the power of the radio and is connected between the radio and antenna. In cases where Ultra4 Australia is the co-promoter using an existing sanctioning body's event logistics, that sanctioning body's radio frequencies will be used. All entrants must verify radio frequencies before attending any event.

## Environmental

1. Ultra4 Australia Pty Ltd maintains utmost concern for all environmental concerns and considerations. Where applicable, local and/or state/federal laws may over-rule this rule book concerning environmental laws.

2. Each vehicle shall carry a 25 litre or larger disposable plastic trash bag (bright in colour – Not black) to extract any soil contaminated in a roll over. Contaminated soil may be left in plastic 25 litre trash bag on side of course for sweep crew to collect and dispose of.

## Series Rules And Regulations

### Series Specific Rules

1. This section reserved for additional rules regarding the points structure for any supplementary events or series associated with a Ultra4 Australia event.

### Infractions & Penalties

1. The Race Director/s, Senior Operations Director, and Chief Technical Inspector have the authority to penalise, disqualify, or suspend any entrant for violations of the technical rules.
2. This rulebook is not meant to infer that these are the only possible infractions, nor the only penalties that may be assessed against any entrant participating in a Ultra4 Australia event.



3. Any entrant disqualified from any event for any reason whatsoever forfeits any and all prize money, points, and contingencies won in that event. Entrant will not be entitled to a refund of any portion of entry fee.

### Protests & Grievances

1. One red card will be presented to each entrant at registration. Each entrant may file up to three complaints to the CRC during the event by using a red card. Red cards are to be presented at the next available checkpoint, or finish line if there are no remaining checkpoints prior to the finish line. Red card complaints may be made against competing entrants for the following infractions during the course of an event:
  - Vehicle leaving the course or short coursing.
  - Receiving outside assistance.
  - Refuelling without an approved fuel mat or tarp acting as an impervious barrier.
  - Unsportsmanlike conduct.
  - Excessive or abusive nerfing or bumping.

- A competitor who believes they have been aggrieved by an item in the rulebook, by the action or inaction of a director, volunteer, or other participant (including other competitors) has the right to file a grievance with the CRC. Such a grievance must be filed with the Race Director no later than 30 minutes after the completion of the event, using the red card system.

## Event Officials

The following are the designated Ultra4 Australia event officials:

### Race Director

1. The Senior Race Director (SRD) of Ultra4 Australia is responsible for the conduct of all business transactions and events of the organisation. All other officials are responsible to the SRD. The SRD, in partnership with the Senior Operations Director, has the final decision on all issues involving any Ultra4 Australia event. The SRD, in partnership with the Senior Operations Director, has full discretion to make any final determinations of, judgments on, or assess penalties with respect to all Ultra4 Australia rules.



### Race Director 2IC

1. An Assistant Officer of Ultra4 Australia sharing responsibility with the Race Director for the on-course conduct of all events. The Race Director 2IC also executes other duties and responsibilities in partnership with the Production Directors.

### Race Official

1. An individual designated by the Race Director to officiate at a Ultra4 Australia event. There are normally several Race Officials for any given event. As a collective group, the term “officials” includes the Directors, the following list of officials, and any other person designated an event official by Ultra4 Australia for a given event.

### Course Marshall

1. The race official appointed by Ultra4 Australia to assist the Race Director in the on-course conduct of an event.

### Chief Technical Inspector

1. The race official appointed by Ultra4 Australia to direct the inspection of entrants' vehicles before and after each event, for compliance with the applicable portions of the Ultra4 Australia Rulebook.

### Assistant Chief Technical Inspector

1. The race official appointed by the Chief Technical Inspector and Ultra4 Australia to assist the Chief Technical Inspector. The Assistant Chief Technical Inspector shall perform all duties of the Chief Technical Inspector in the event the Chief Technical Inspector is not available. The Assistant Chief Technical Inspector shall carry out all other duties assigned by the SRDs, Race Director, and/or Chief Technical Inspector.

### Timekeepers

1. The race official appointed by Ultra4 Australia to direct the timing and scoring operations of an event.

### Checkpoint Marshal



1. A race official appointed by Ultra4 Australia to direct the operations of a particular checkpoint and the immediate area around that checkpoint.
2. Checkpoint captains are the direct representatives of the Race Director 2IC at their respective checkpoints. Their area of responsibility includes 50 Meters on either side of their checkpoint.
3. Checkpoint captains will designate areas leading to and surrounding their checkpoint area. These areas shall be for checkpoint personnel only. No support teams, pit crews, chase crews, nor any other person not having express permission, will be permitted in these areas. Failure to comply will subject the offending entrant or team to penalties of up to and including disqualification.

### Road-Crossing Marshal

1. A race official appointed by Ultra4 Australia to direct the operations of a particular road-crossing and the immediate area around that road-crossing.

### Pit Stop Marshal

1. A race official appointed by Ultra4 Australia to direct the operations of a particular pit stop and the immediate area around that pit stop.

### Competition Review Committee (CRC)

1. A panel composed of the Senior Race Director, Senior Operations Director, Race Director, Chief Technical Inspector, and the CRC Director. The CRC shall meet as required to decide matters concerning rule infractions, sportsmanship, and on-course conduct; primarily those identified by on-course officials and the red-card system. The CRC shall meet approximately 30 minutes after the close of the event finish line, and before the event awards ceremony.

### CRC Director

1. Director of the CRC, appointed by the Production Directors. The CRC Director shall be a third-party holding no position in Ultra4 Australia. Ultra4 Australia shall make the CRC Director known to the event participants at the drivers' meeting, and he shall represent the drivers in post-race rule matters. CRC Director will oversee the CRC process, call the meeting



to order, and report the outcome of the CRC to the event participants.

## Appendix A - Ultra4 Australia Rulebook - Glossary Of Terms, Acronyms, And Abbreviations

### Automatic Disqualification

1. Disqualification assigned immediately to an entrant, whether or not they have completed the event, by the SRD, Senior Operations Director, or Race Director, at the discretion of Ultra4 Australia for certain grievous infractions. Entrants suffering automatic disqualification shall be so informed by the SRD, Senior Operations Director, or Race Director and, when so informed, must immediately leave the course by the safest and most expeditious means possible without interfering with any other entrants still in competition. Failure to comply shall result in fines and/or suspension.

### Co-Driver

1. A person who is listed on an official Ultra4 Australia event entry form as the co-operator of a vehicle and who is eligible to ride in a vehicle during the course of the event. The Co-driver must sign all entry and release forms in person during the normal registration time at the



same time as the Nominated Series Driver is signing all entry and release forms.

### Competitor

1. The person or persons occupying the vehicle during an event. The Nominated Series Driver and the Co-driver(s) (if present) are the competitors.

### Contingency

1. Contingency is a commitment made by a manufacturer, retail outlet, or other business to Ultra4 Australia regarding a specific purse to be paid to event competitors in return for display of contingency logos. Event competitors must sign up for contingency with manufactures, and manufactures must execute contracts with competitors immediately following the completion of the event. Contingency offers must be made available to Ultra4 Australia no less the 30 days before the start of any event for which the contingency is offered.



## Crew Member

1. Any person associated with, acting on behalf of, or working for a team, whether compensated or not, is a crew member. Crew members include, but are not limited to: pit crew, radio operators, refuellers, mechanics, volunteers, support personnel, cooks, cleaners, cheerleaders, umbrella girls, etc.

## Designated Time Limit

1. The maximum allowable time entrants have to complete the course/event or cross the finish line. Calculated as the total elapsed time between the entrant's start time and either:
2. The time at which the finish line closes or the time at which the checkpoint immediately after the entrant's present position closes, whichever occurs first.

## Disqualification

1. The state of being disqualified. An official event result assigned to entrants for certain violations of the rules. Disqualification results in the entrant being ineligible for any points, purse, prize or contingency related to the event in which they are disqualified. Entrants who



are disqualified receive no finishing time, score, or placement for the event in which they are disqualified. Disqualification may be decided post-event by the CRC, or, for certain grievous infractions, may be assigned immediately and automatically to an entrant by the SRD, Senior Operations Director, or Race Director, at the discretion of Ultra4 Australia. The latter case is known as automatic disqualification.

## DNF

1. Did Not Finish – The official event result assigned to entrants that do not complete the course/event or do not complete the course/event within the maximum allowable time.

## DNS

1. Did Not Start - The official event result assigned to entrants that do not start the course/event, whether unable to or prohibited from doing so by regulation or ruling.

## Nominated Series Driver

1. A person who is listed on the official Ultra4 Australia documents as the main operator of a vehicle during the series. The Nominated Series Driver must sign all Ultra4 Australia. forms

and is responsible for any taxes that may be incurred from prize money resulting from event purse, prizes, or contingency. The Nominated Series Driver must be at least 18 years of age at the start of an event, and must be at the controls of the vehicle at the start or finish of any Ultra4 Australia. event in which they are listed as such. The Nominated Series Driver must sign all entry and release forms in person during the normal registration time. Only ONE (1) Nominated Series Driver will be assigned to a race number per season.

### Drivers' Meeting

1. A pre-competition meeting chaired by the Directors at which critical and specific event information is provided to competitors. Attendance at the drivers' meeting is compulsory.

### Entrant

1. An entity competing in a series and consisting of a registered vehicle, a registered Nominated Series Driver, and from zero to three registered Co-drivers.

### Event

1. A contest or competition between competitors driving vehicles.



### Excessive/Abusive Nerfing or Bumping

1. Deliberately contacting another vehicle with the intent to damage, disrupt, or upset that vehicle or its occupants. Deliberately contacting another vehicle with wilful disregard for the safety of the other vehicle's occupants or the integrity or condition of the other vehicle or possible damage that such contact may cause to the other vehicle.

### Finish of The Event

1. The finish of the event is defined as the time at which the last competitor completes the course/event or crosses the finish line; or the time at which the finish line/last checkpoint closes, whichever occurs first.

### Ultra4 Australia

1. A Queensland, Australia entity, formed for the purpose of organising, producing, and promoting off-road motor-sport events, races, and films.

### Ultra4 Australia Rulebook

1. The manual published by Ultra4 Australia containing the rules for the conduct of, and participation in, Ultra4 Australia events.

### Impound

1. A specific place, with restricted access, to be used for the purpose of the pre-race and/or post-race inspection of a vehicle.

### Infraction

1. Failure to comply with, adhere to, obey, or carry out any rule, regulation, practice, procedure, requirement, or specification contained in the Ultra4 Australia Rulebook, including any Special Rules or Supplementary Regulations.

### Outside Assistance

1. Any physical act, action, or other help received by any entrant from any person who is not a registered member of that entrant, other than in designated pits.

### Participant



1. Any person, in any capacity, taking part in a Ultra4 Australia event. Competitors, their crew members and support personnel, and team's sponsors are all participants.

### Pit Crew

1. A crew member, or members, who specifically work in the pits during an event.

### Purse

1. Prize money awarded in accordance with the specific guidelines for a particular event. Event purse shall be announced at drivers' meeting and be paid out following event.

### Qualifier

1. An event in which competitors compete to qualify for a starting position in the main race.

### Race

1. A contest of speed and/or endurance, against the clock and/or other vehicles. A race is a specific type of event.

## Red Card

1. A ticket, token, card or other device provided to each driver before the event which they may use to report a violation of the rules that they witness or to file a grievance with the Directors or CRC.

## Rules

1. Rules, regulations, specifications, and guidelines published in the Ultra4 Australia Rulebook for the purpose of ensuring the safe, fair, and orderly conduct of races and other events.
2. Any Team member that requires a copy of the Ultra4 Australia Official rulebook from any Ultra4 Australia staff member during an event to aid personal verification of said rules, or to dispute rules either before, during or after the event with the CRC board, will be required to pay a \$50 upfront fee for a hard copy of the Ultra4 Australia Rule book.

## Short Coursing

1. Any deviation from the marked course for any reason – apart from where directed by a



## Special Rules

1. Rules issued by Ultra4 Australia to account for conditions presented by the location of the event or race, the condition of the course or any other circumstance.

## Stand-Alone Event

1. An event promoted independently by Ultra4 Australia without the cooperation or collaboration of any other sanctioning or promotion company or body.

## Start of The Event

1. The start of the event is defined as the time at which the drivers' meeting begins.

## Stuck Stub

1. A card, marker, token, or other device that uniquely identifies an entrant and that, in the event of an entrant becoming disabled or broken down, is given by its owner to a competing entrant so that the competing entrant may carry it and deliver it to the next available official

in order to indicate to the official that the original owner of the stuck stub is broken down or disabled and no longer in competition in the event.

### Supplementary Regulations

1. Rules set forth by Ultra4 Australia that are supplemental (in addition) to the rules contained in the Ultra4 Australia Rulebook, or that amend, suspend or modify existing rules and regulations. Supplementary Regulations apply to a specific event, race, venue, or time and place.

### Team

1. A person or persons, identified by a team name, and associated with a particular, specific, and registered Nominated Series Driver and a vehicle that bears an official entrant number, constitute a team. Often a team will be composed of a Nominated Series Driver, Co-driver(s), various crew, and possibly sponsor representatives. However, in the case of a single-seat race vehicle, it is possible that a single person, (the Nominated Series Driver) and a registered vehicle constitute the whole team.



### Technical Rules

1. Those rules, regulations, and specifications set forth in the Ultra4 Australia Rulebook that govern the technical, design, and equipment requirements for vehicles, competitors, pit crews and support crews competing in Ultra4 Australia events.

### Unsportsmanlike Conduct

1. Conduct incompatible with internationally accepted general principles of fair play and sportsmanship, as judged by Ultra4 Australia